



CITY OF
SIOUX FALLS

Tier Map Amendments

IRAB: March 4, 2026

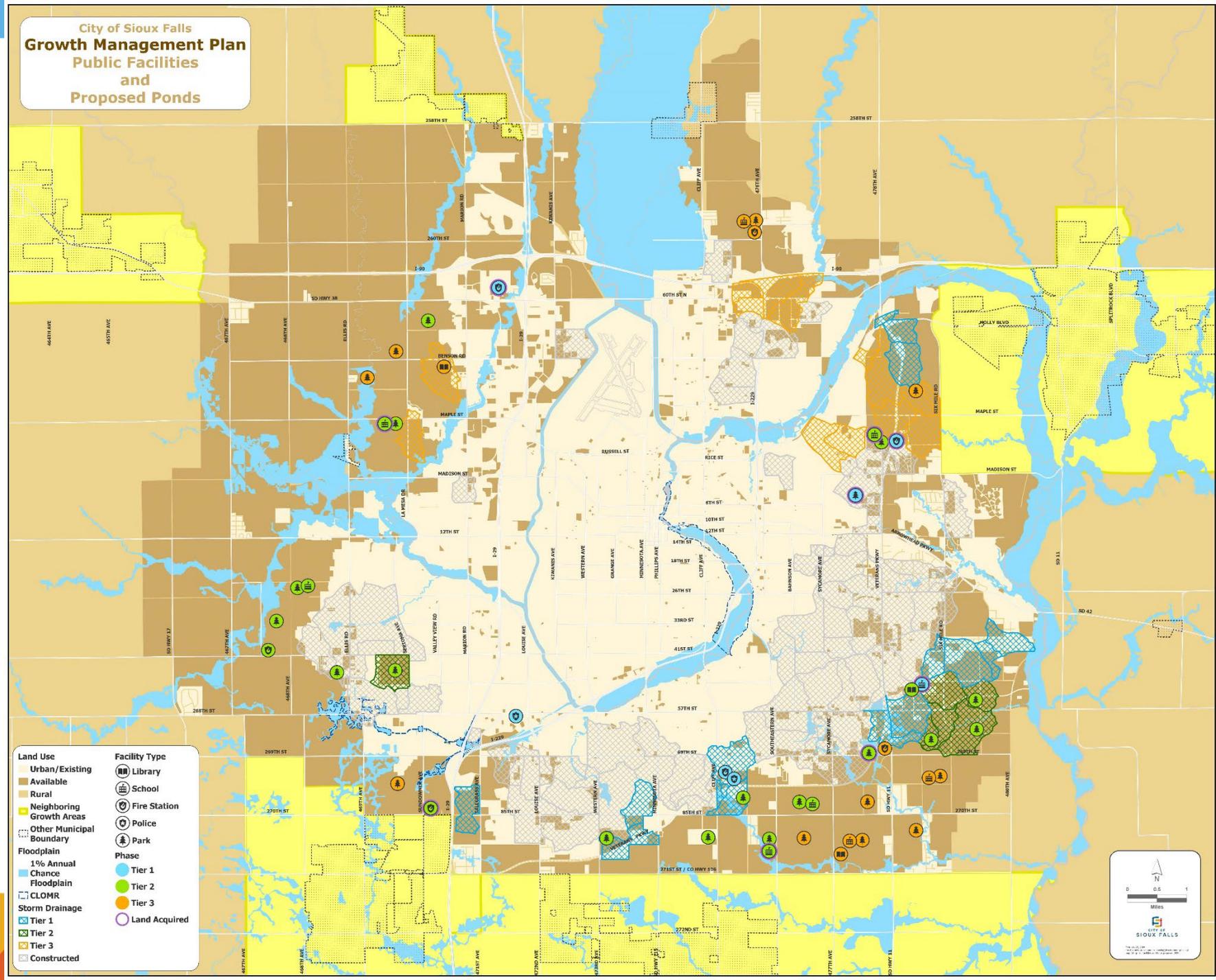
Shape Sioux Falls Comprehensive Plan

- Shape Sioux Falls 2040 adopted in 2016. Major amendment approved on February 10, 2026 to adopt **Shape Sioux Falls 2050**.
- *“The map should be updated each year to keep the citizens and development community updated as to the areas of the city that are available for urban services and development of the Growth Management Policy Framework.*
For growth management to be successful, policies guiding the provision of urban services are needed. The growth management policy framework is intended to focus and direct the actions of the City into a consistent, workable growth management approach. “ p. 53

Tier Maps

- Tier 1: Annexation advised within five years. All City services available within the five-year CIP period.
 - Tier 2: Annexation not advised. City services are projected to be available for development within 6 to 15 years (additional monetary resources needed).
 - Tier 3: Annexation not advised. City services are projected to be available for development within 16 to 25 years (additional monetary resources needed).
-
- Development Areas – 5A [overall compatibility map]
 - Sanitary – 5B
 - Water_ Roads – 5C
 - Drainage_ Public Facilities - 5D

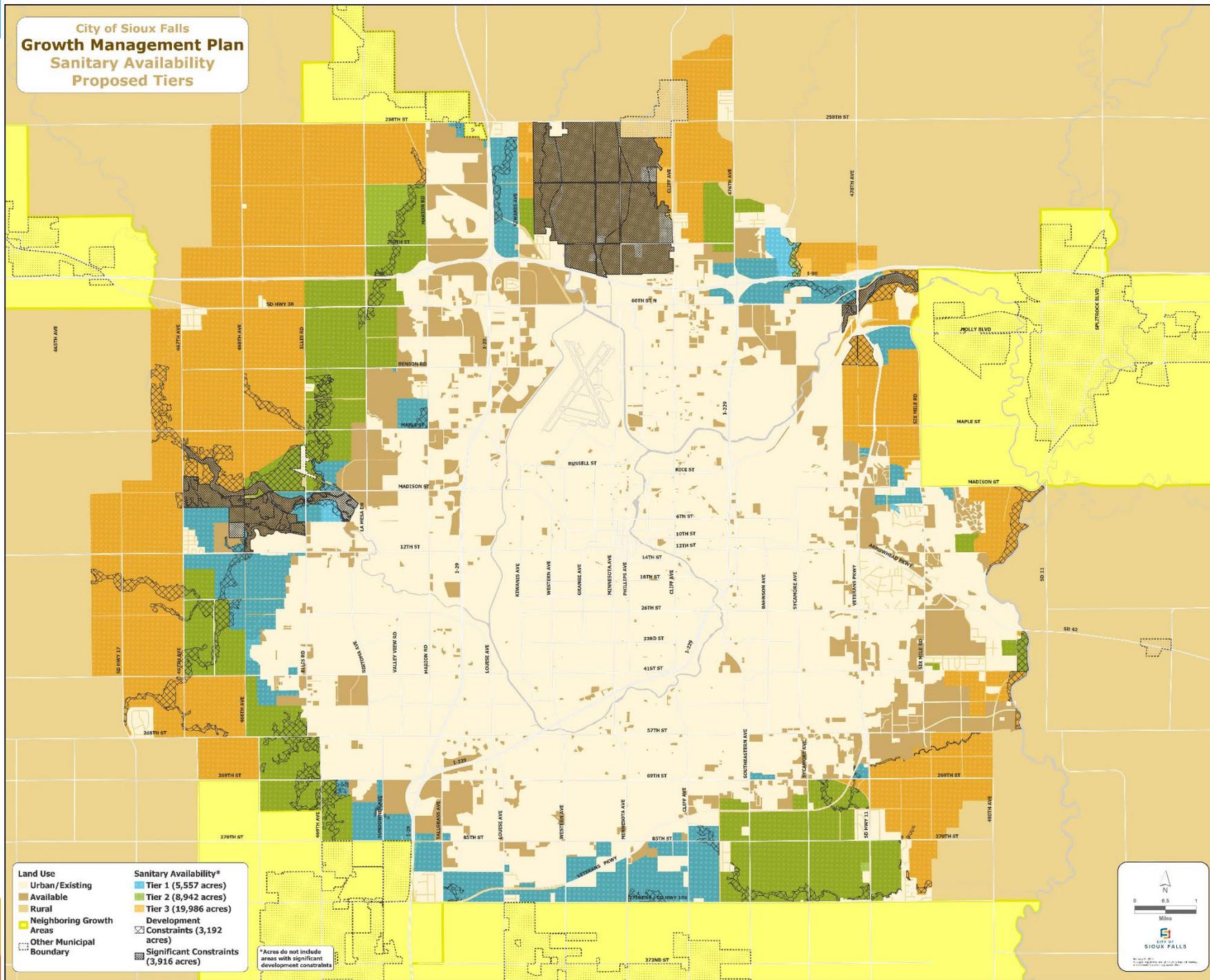
City of Sioux Falls Growth Management Plan Public Facilities and Proposed Ponds



Land Use	Facility Type
Urban/Existing	Library
Available	School
Rural	Fire Station
Neighboring Growth Areas	Police
Other Municipal Boundary	Park
Floodplain	Phase
1% Annual Chance Floodplain	Tier 1
CLOMR	Tier 2
Storm Drainage	Tier 3
Tier 1	Land Acquired
Tier 2	
Tier 3	
Constructed	



**City of Sioux Falls
Growth Management Plan
Sanitary Availability
Proposed Tiers**



- | | |
|--------------------------|---------------------------------------|
| Land Use | Sanitary Availability* |
| Urban/Existing | Tier 1 (5,557 acres) |
| Available | Tier 2 (8,942 acres) |
| Rural | Tier 3 (19,986 acres) |
| Neighboring Growth Areas | Development Constraints (3,192 acres) |
| Other Municipal Boundary | Significant Constraints (3,916 acres) |

*Acres do not include areas with significant development constraints

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CITY OF SIOUX FALLS

Land Use Demand

Exhibit 2B:

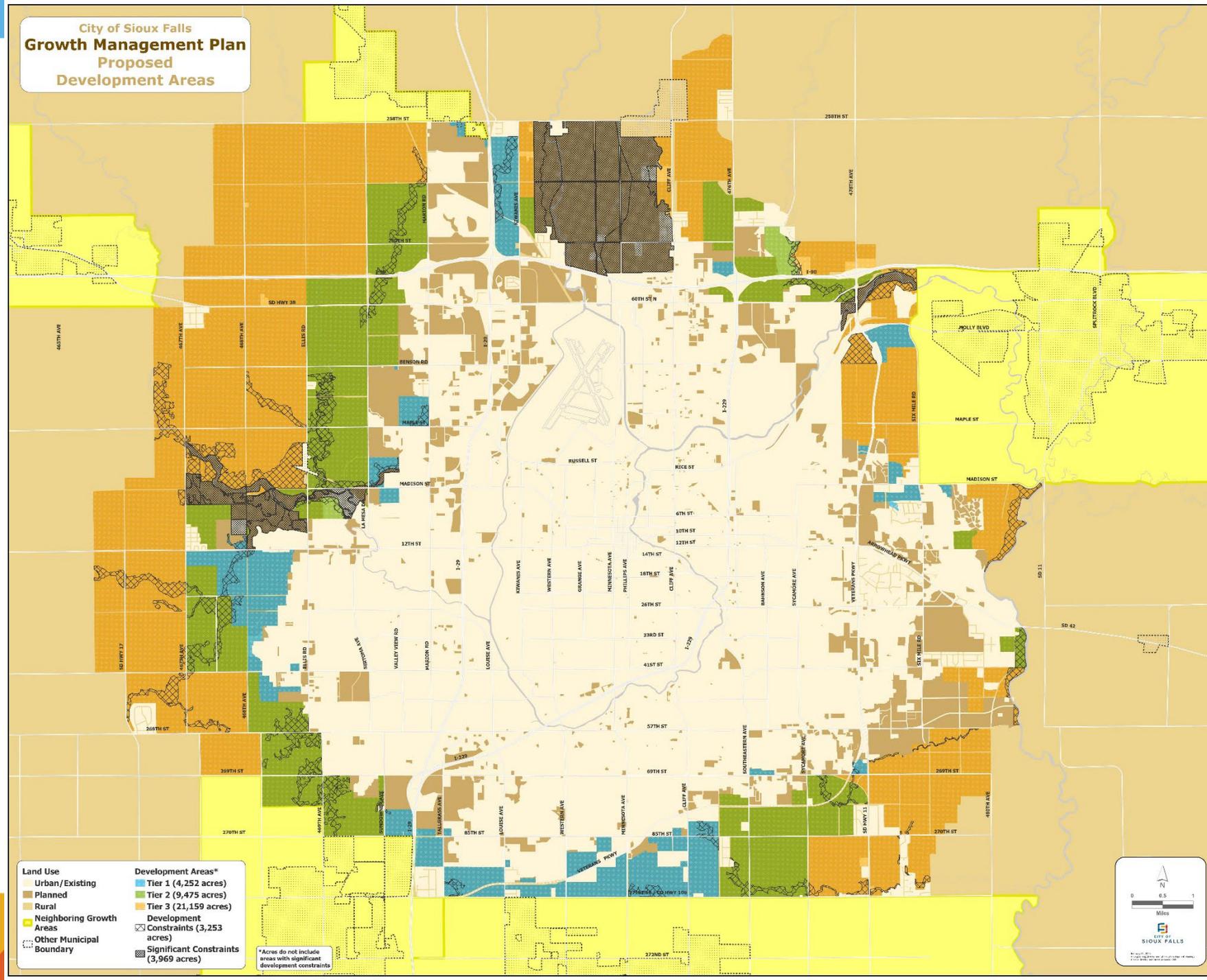
Developable Platted Lands	Actual 2009*	2019	2020	2021	2022	2023	2024	10 yr. AVG.	10 yr. Median
Residential	534	582	489	774	351	344	260	459.4	420.0
Office	13	142	134	178	172	77	39	121.4	140.3
Commercial	124	278	57	101	68	55	67	96.5	67.5
Industrial	48	191	224	436	293	242	174	215.5	207.5
Subtotal	719	1,193	904	1,489	884	718	540	892.8	835.2

* Jan. 1, 2009 Platting Fees effective

Exhibit 2D: 2050 GMP, 26-year Period

Single Family	24,034	924.4
Multi Family	2,116	81.4
Office	2,600	100
Commerical	1,820	70
Industrial	5,200	200
Public / Semi Public*	1,750	67.3
Total	37,521	1,443

**City of Sioux Falls
Growth Management Plan
Proposed
Development Areas**



Land Use	Development Areas*
Urban/Existing	Tier 1 (4,252 acres)
Planned	Tier 2 (9,475 acres)
Rural	Tier 3 (21,159 acres)
Neighboring Growth Areas	Development Constraints (3,253 acres)
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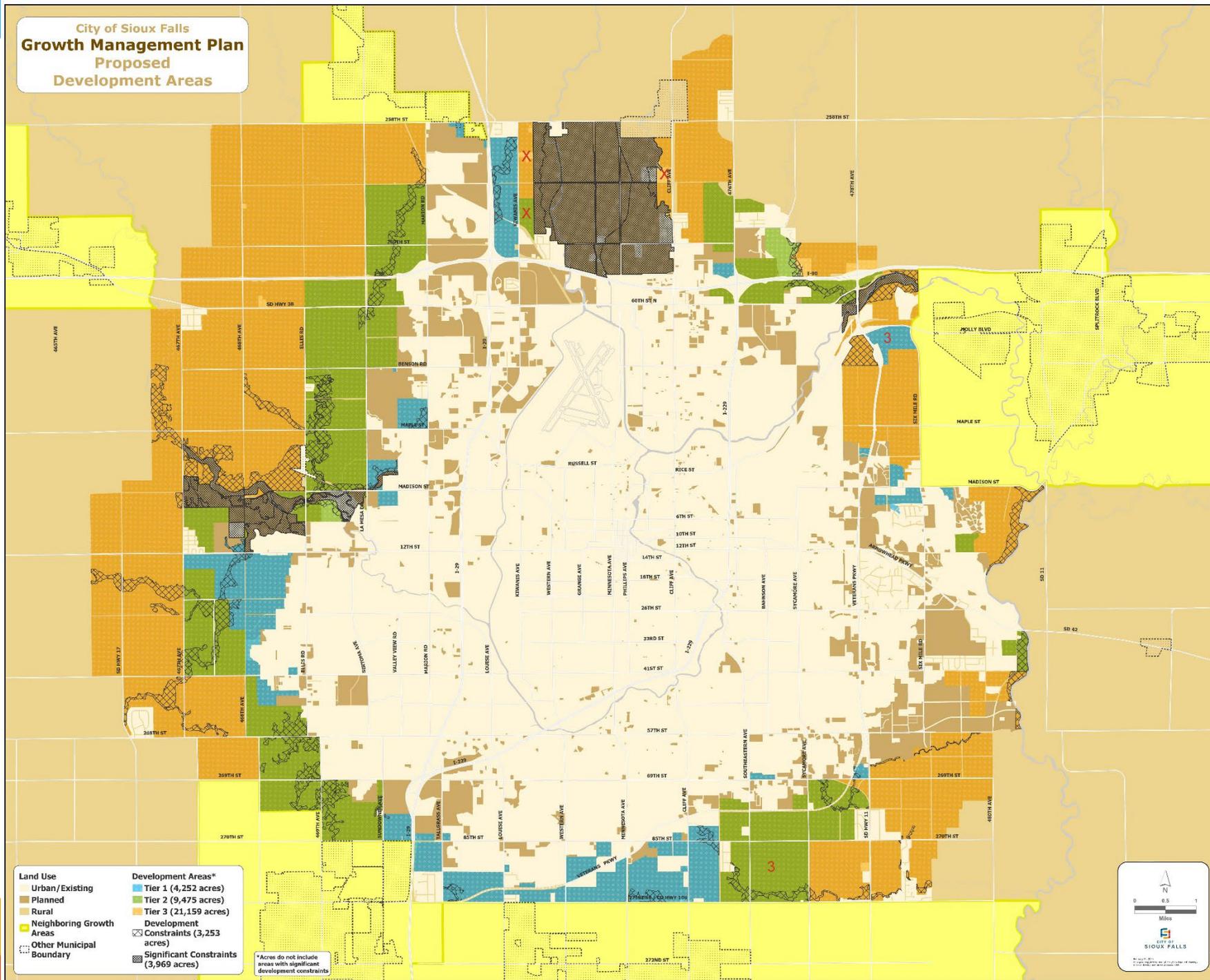
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CITY OF SIOUX FALLS

Map prepared by the City of Sioux Falls Planning Department. The City of Sioux Falls is not responsible for the accuracy of the information shown on this map. The City of Sioux Falls Planning Department reserves the right to change the information shown on this map without notice.

**City of Sioux Falls
Growth Management Plan
Proposed
Development Areas**



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CITY OF SIOUX FALLS

PLANNING DEPARTMENT
1000 WEST WASHINGTON AVENUE
SIOUX FALLS, SD 57105
765.2711

Tier Maps - Process

- Working to get maps approved by City Council in May.
- Effective by June.
- To be used / available within the September Capital Improvement Program- City Council's public hearing process.

- Then staff will review, update, amend maps (again) in Fall 2026.
 - Starting the annual review process, with updated utility information.
 - What's been completed, designed, funded, delayed...
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- Planning – (605) 367-8888
- Engineering – (605) 367-8601
 - Sanitary Sewer, Water, Street Construction, Drainage

Thank You.



CITY OF
SIOUX FALLS

Tier Map Amendments

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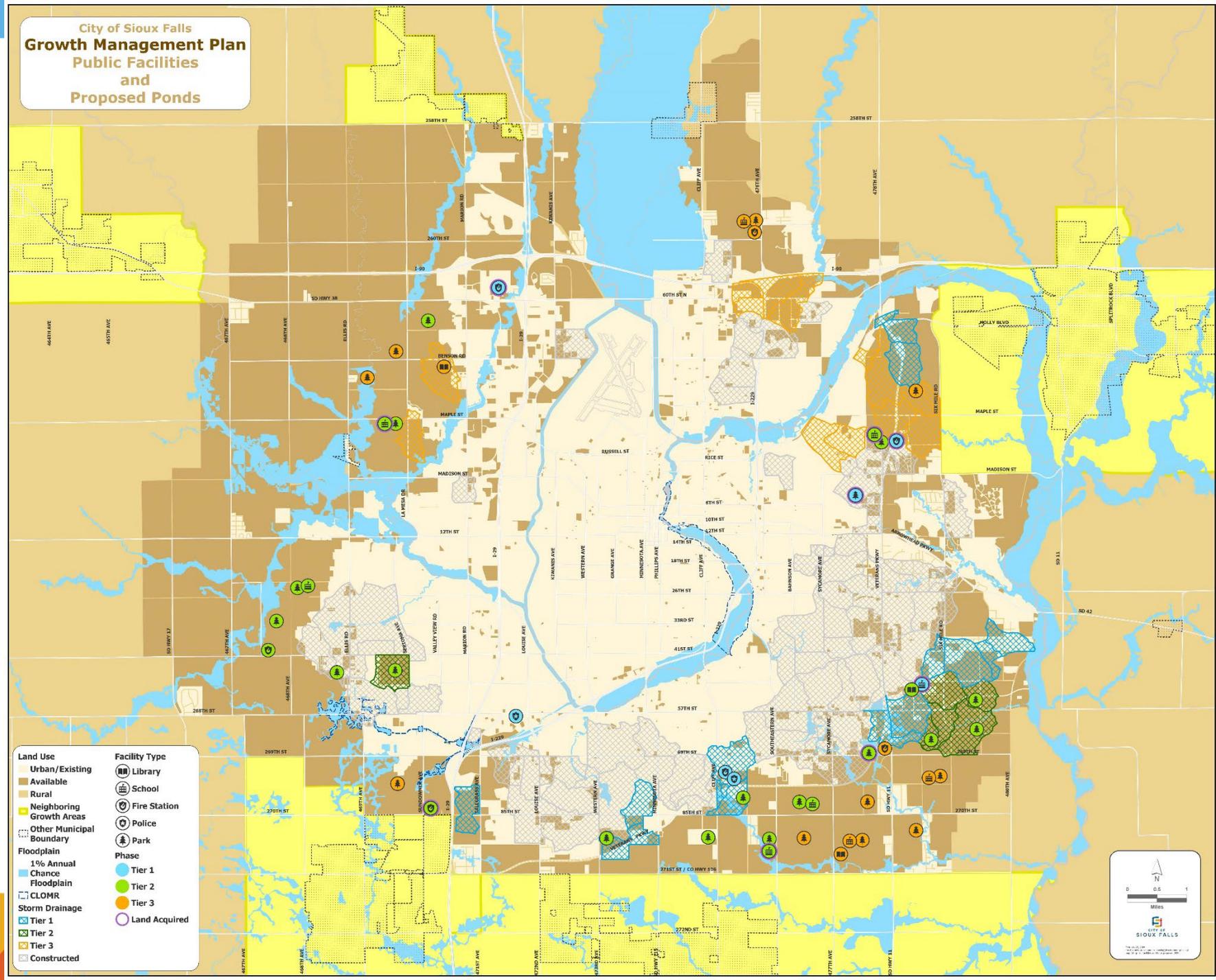
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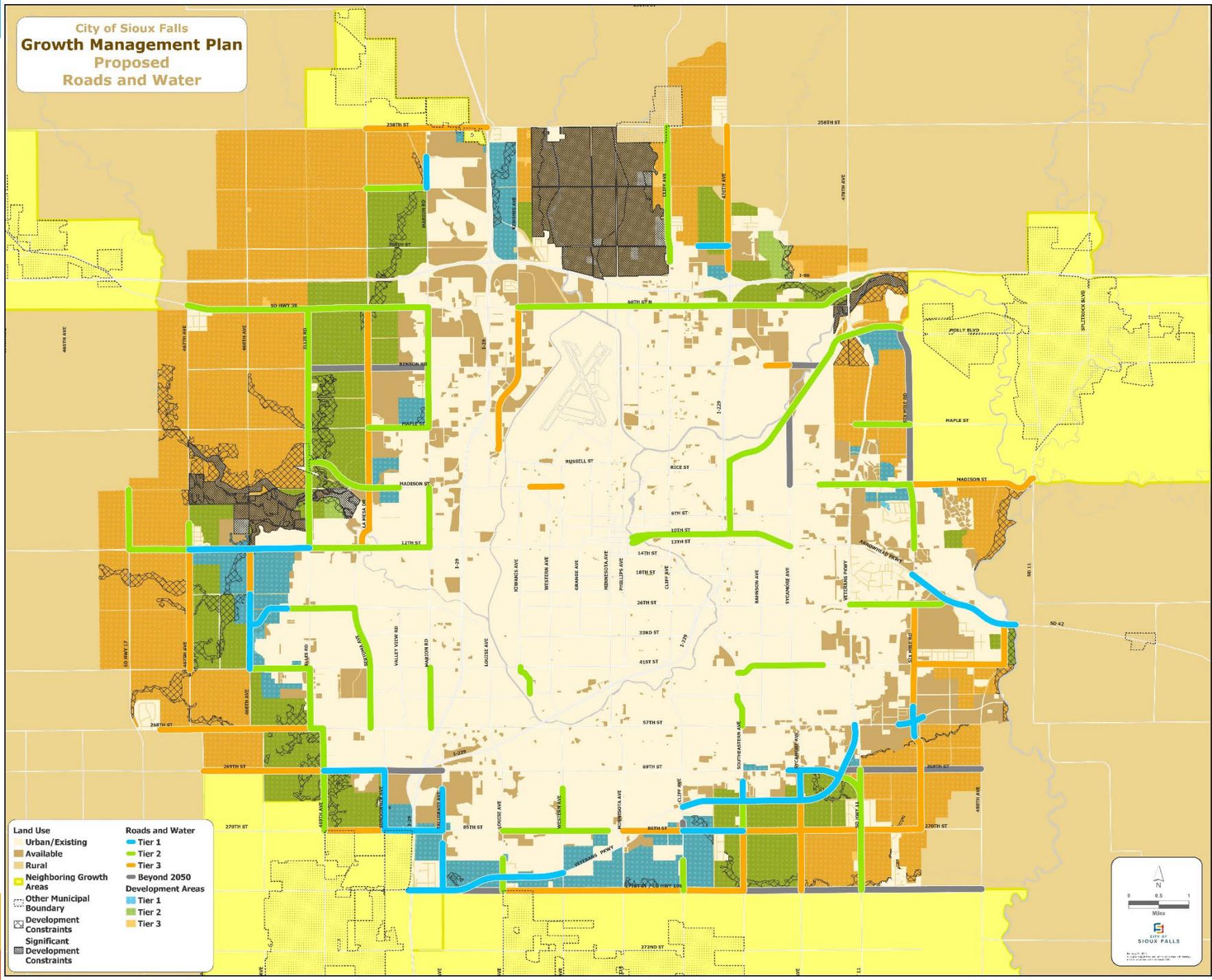
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**City of Sioux Falls
Growth Management Plan
Proposed
Roads and Water**



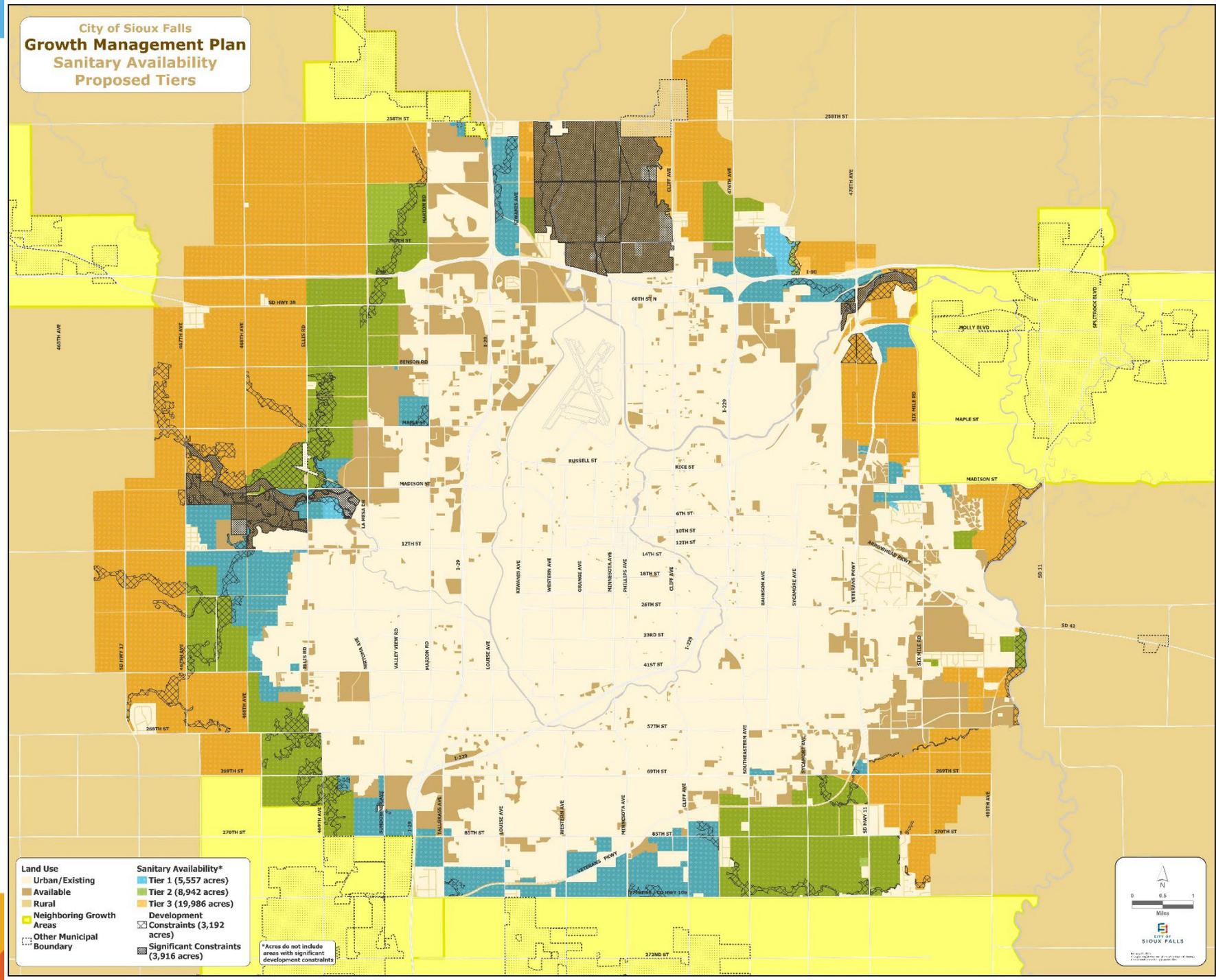
Land Use	Roads and Water
Urban/Existing	Tier 1
Available	Tier 2
Rural	Tier 3
Neighboring Growth Areas	Beyond 2050
Other Municipal Boundary	Development Areas
Development Constraints	Tier 1
Significant Development Constraints	Tier 2
	Tier 3

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CITY OF SIOUX FALLS

PLANNING & DEVELOPMENT
1000 WEST BROADWAY, SUITE 1000
SIOUX FALLS, SD 57105-5000
765.271.1000

**City of Sioux Falls
Growth Management Plan
Sanitary Availability
Proposed Tiers**



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CITY OF
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Land Use Demand

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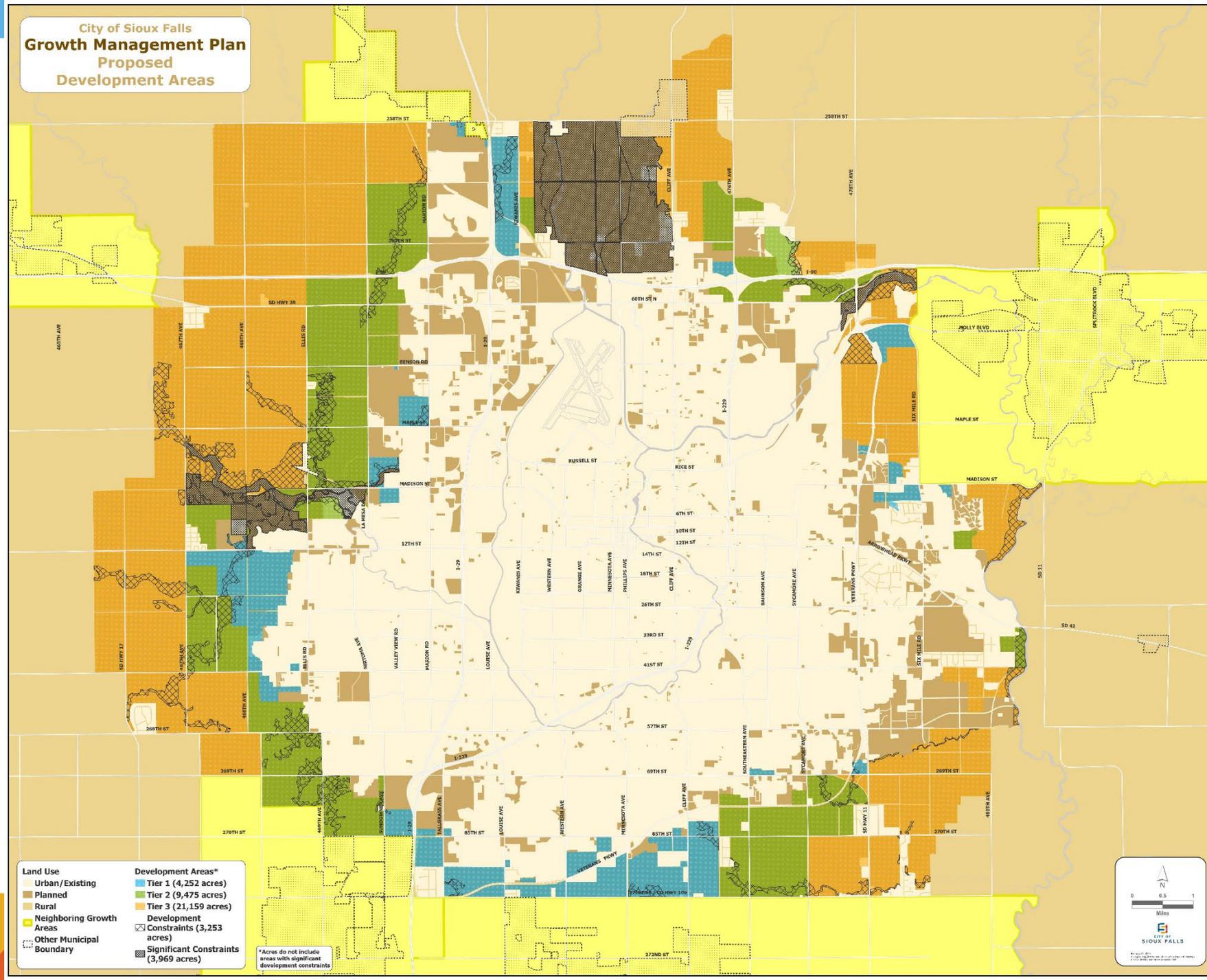
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**City of Sioux Falls
Growth Management Plan
Proposed
Development Areas**



- Land Use**
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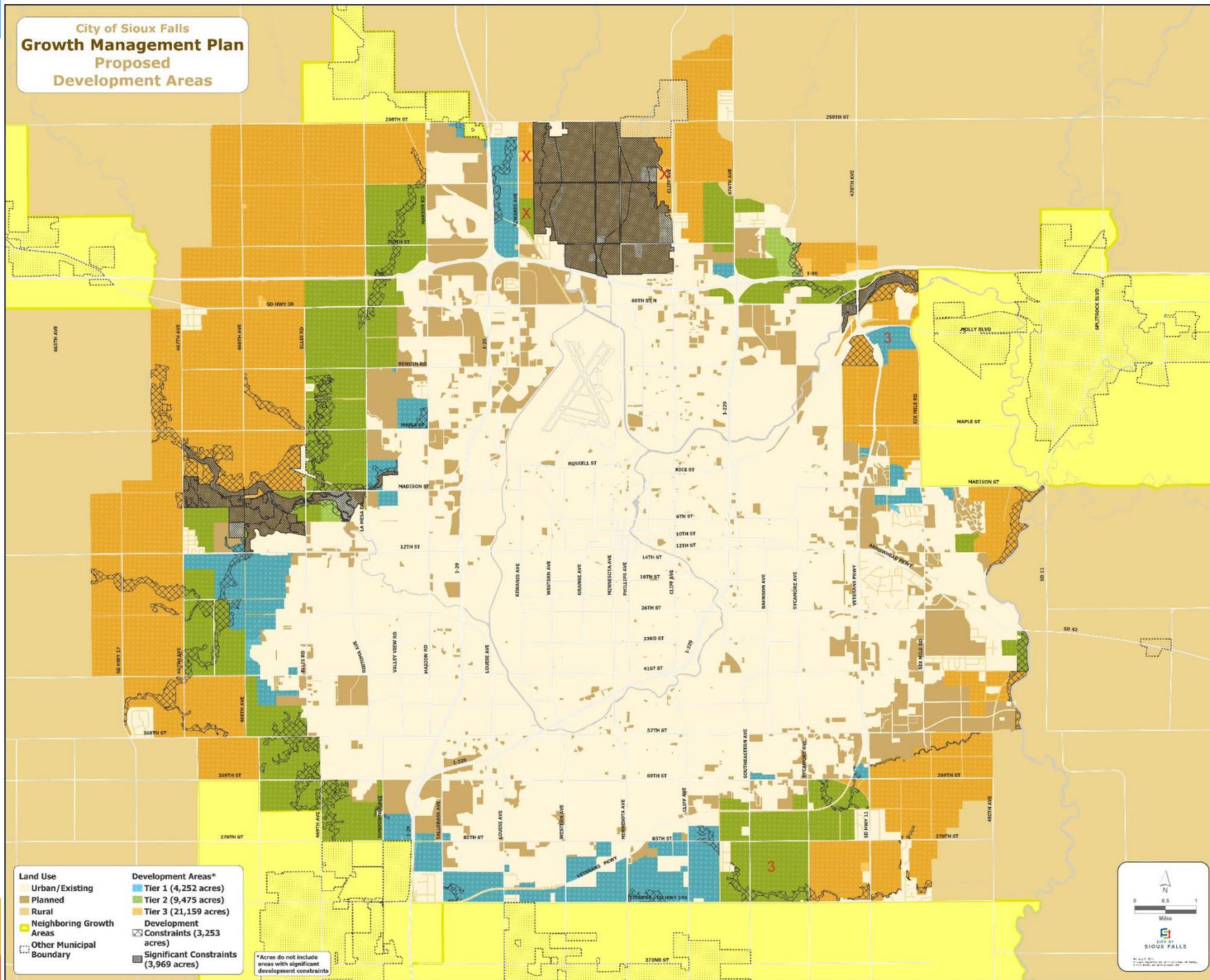
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CITY OF SIOUX FALLS

PLANNING & DEVELOPMENT
2024

**City of Sioux Falls
Growth Management Plan
Proposed
Development Areas**



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CITY OF SIOUX FALLS

PLANNING DEPARTMENT
1000 WEST BROADWAY, SUITE 100
SIOUX FALLS, SD 57105-5000
765.233.3333

Tier Maps - Process

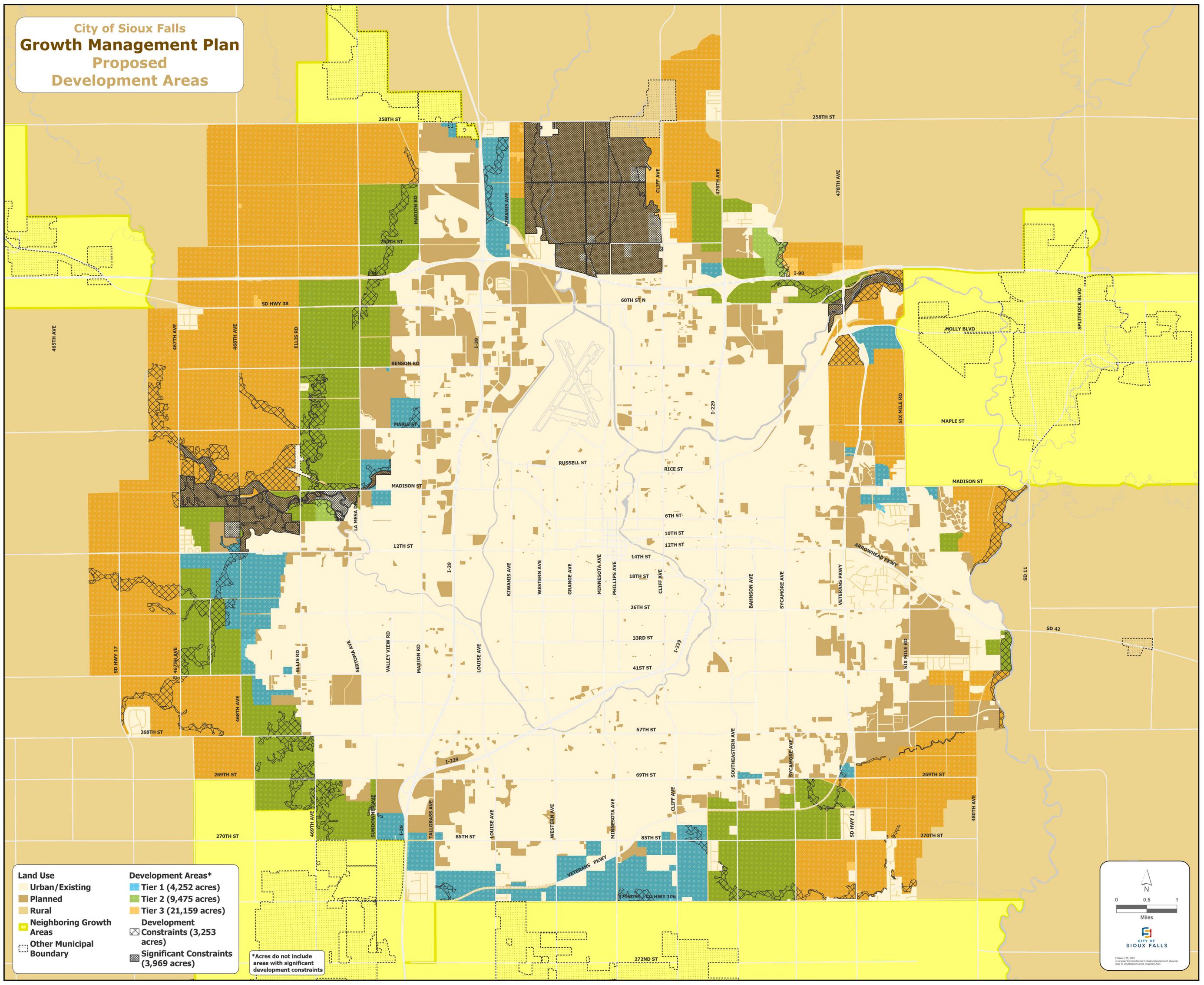
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City of Sioux Falls
Growth Management Plan
 Proposed
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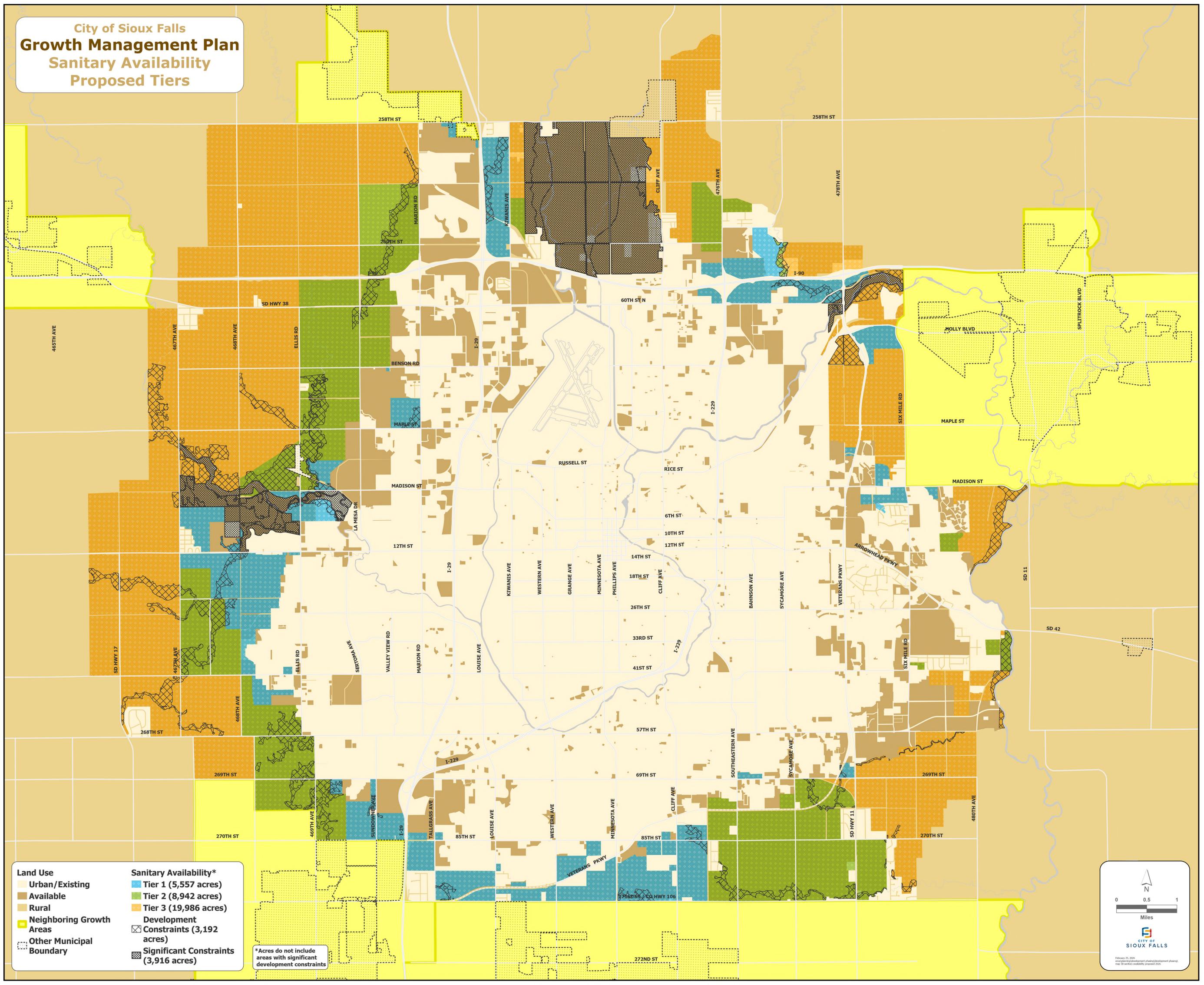
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CITY OF SIOUX FALLS

February 25, 2020
 final planning/development plan/planing/development plan/ing
 map for development area proposed 2020

City of Sioux Falls
Growth Management Plan
Sanitary Availability
Proposed Tiers



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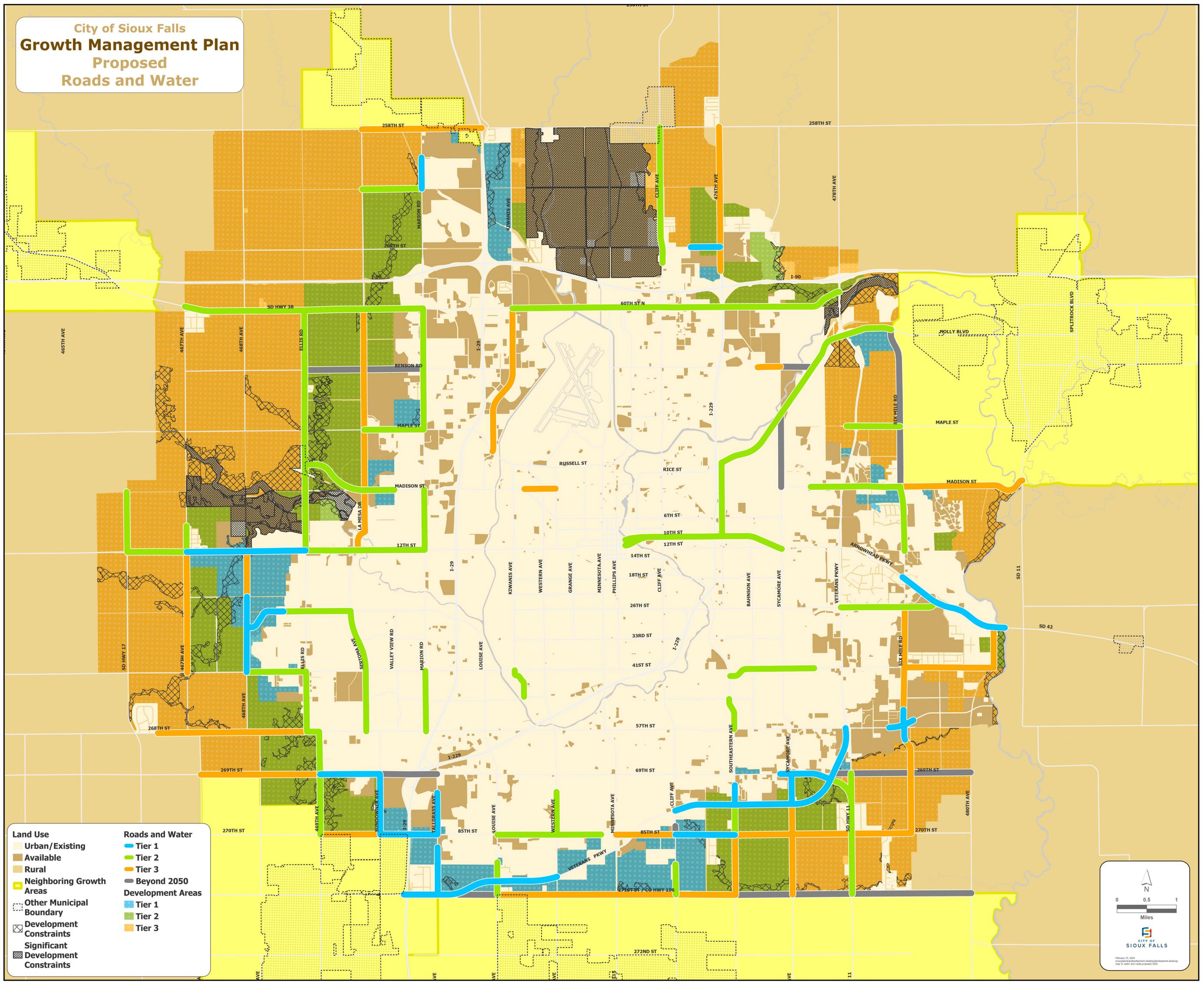
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CITY OF SIOUX FALLS

February 23, 2024
revised planning/development planning/development planning
map ID sanitary availability proposed 020

City of Sioux Falls
Growth Management Plan
Proposed
Roads and Water



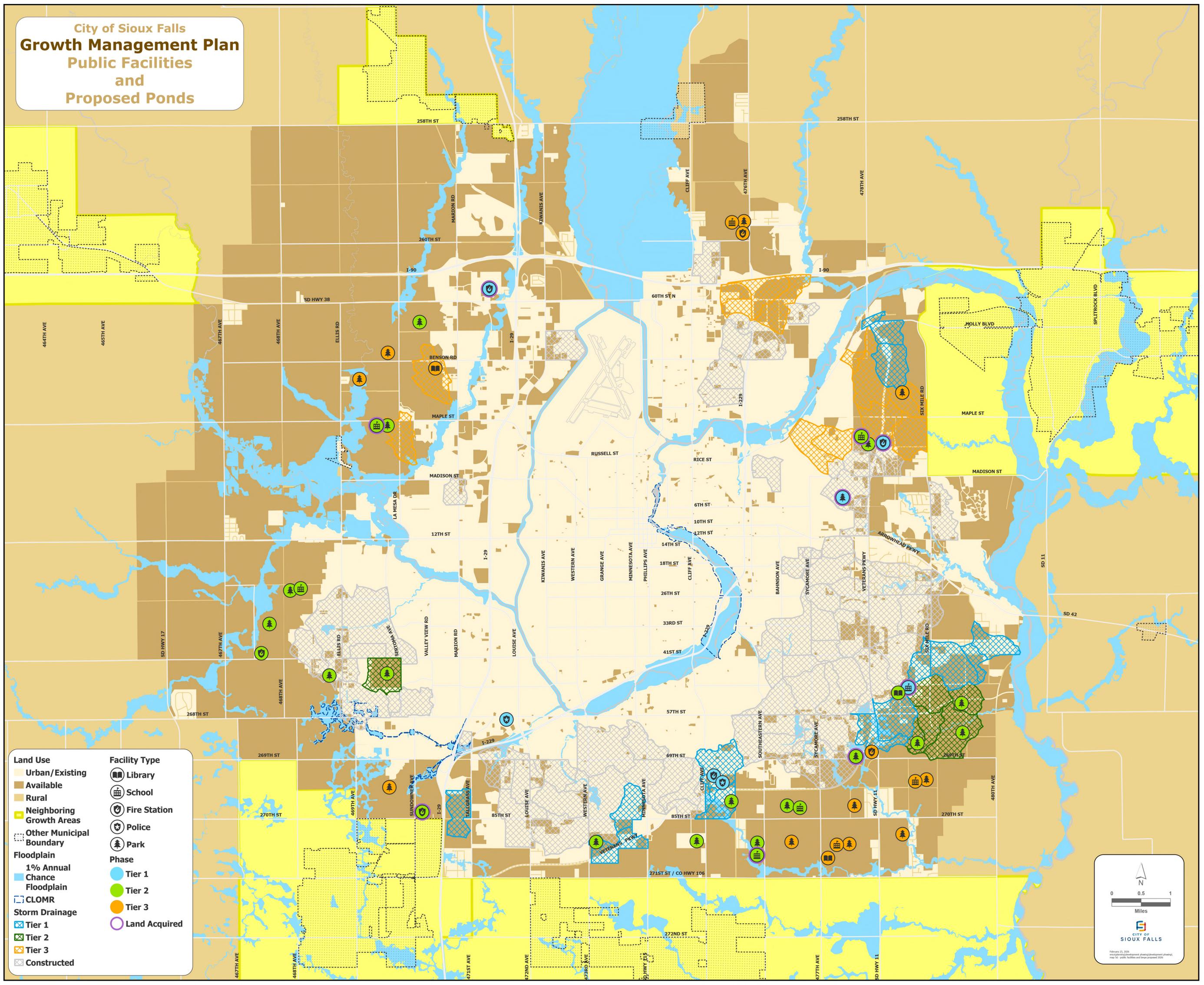
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Miles

CITY OF SIOUX FALLS

February 20, 2020
revised planning/development planning/development planning
map 15 water and roads (revised 2020)

City of Sioux Falls
Growth Management Plan
Public Facilities
and
Proposed Ponds



Land Use	Facility Type
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CITY OF SIOUX FALLS

February 20, 2020
www.growthmanagement.phoenixdevelopment.com
Map 51 - Public Facilities and Storm Drainage 2020

Chapter 5
Street Access and Parking Lot Criteria

Chapter 5

Street Access and Parking Lot Criteria

Section	Topic	Page
5.1	Traffic Studies	5.1
5.2	Access Control	5.6
5.3	Access Design	5.13
5.4	Off-Street Parking Area	5.17
5.5	On-Street Diagonal Parking In Urban Villages	5.18

Chapter 5

Street Access and Parking Lot Criteria

5.1 Traffic Studies

5.1.1 Responsibilities for Traffic Report

5.1.1.1 Traffic ~~impact~~ reports may be required by the City ~~in order~~ to adequately assess the impact of a proposal on the existing and/or planned street system. Traffic Reports are defined as a Traffic Impact Study or a Traffic Memorandum. A Traffic Impact Study is defined as a full traffic analysis following the guidelines listed in this chapter. A Traffic Memorandum is generally an update to an already approved Traffic Impact Study and is considerably shorter in length. The developer shall have primary responsibility for assessing traffic impacts associated with a proposed development, subject to City review. ~~The primary responsibility for assessing the traffic impacts associated with a proposed development will rest with the developer with the City serving in a review capacity.~~

5.1.1.2 Unless waived by the City Engineer, a written report meeting the City guidelines will be required for a nonresidential development proposal when trip generation during the peak hour is expected to exceed 100 vehicles, or any multifamily residential development with 150 or more dwelling units, or any development that is proposing to complete a future land use amendment.

5.1.1.3 Preparation of the report shall be the responsibility of the developer and must be prepared by a South Dakota licensed ~~design~~-professional engineer with experience in transportation planning. Upon submission of a draft traffic report, the City will review the study data sources, methods, and findings. Comments will be provided in a written form. The developer and his engineer will then have an opportunity to incorporate necessary revisions prior to submitting a final report. All reports must be reviewed by the City before acceptance.

5.1.1.4 All previous traffic reports relating to the development that are more than two years old must be updated, unless it is determined that conditions have not changed enough to warrant an update.

The developer will be required to submit a new traffic report if, after submitting the original traffic report, the land use intensity and traffic generation area increased.

5.1.1.5 Traffic reports will be required if the trip generation/dwelling unit criteria as noted in Section 5.1.1.2 are exceeded for the following submittals:

a. For a rezoning application or Conditional Use Permit.

~~a.b.~~ For a final plan or final development plan if the property has already been rezoned for the proposed use and no traffic report was required for the rezoning.

~~b.c.~~ Prior to issuance of a building permit, if the property has already been zoned/platted, and no previous traffic report less than two years old exists.

~~e.d.~~ An Aadditional access off from an arterial street to an existing use is being requested.

~~d.~~ The developer will be required to submit a new traffic report if, after submitting the original traffic report, the land use intensity and traffic generation area increased.

e. A land use amendment is required.

5.1.1.6 Where access points are not defined or a site plan is not available at the time the traffic report is prepared, additional traffic work may be required when a site plan becomes available or the access points are defined.

5.1.1.7 The developer ~~will~~ may be notified at the preplanning stage if a traffic report ~~will be~~ is required, provided sufficient information is available for the City to determine whether the trip generation/dwelling unit criteria have been met. If insufficient information is available but the property appears to involve higher density or higher trip generation land use ~~a sufficiently intense land use~~, the applicant will be informed that a traffic report is required.

5.1.2 Traffic Report Format

Traffic consultants are encouraged to discuss projects with the City prior to starting the study. Topics for possible discussion ~~at such meetings~~ might include directional distribution of traffic, definition of the study area, intersections requiring capacity analysis, and methods for projecting build-out volume. This should provide a firm base of cooperation and communication between the City, the owner or developer, and ~~his consultant~~ his professional engineer in creating traffic characteristics that are in the best interest of the ~~total~~ community. Specific requirements will vary depending on the site location. However, all traffic reports shall contain, ~~at~~ as a minimum, the following information:

5.1.2.1 Introduction

5.1.2.1.1 Land Use, Site, and Study Area Boundaries ~~.-~~ A brief description of the size of the land parcel, general terrain features, the location within the jurisdiction and the region should be included in this section. In addition, the roadways that ~~afford~~ provide access to the site, and are included in the study area, should be identified.

The exact limits of the study area should be based on engineering judgment, and an understanding of existing traffic conditions at the site. In all instances, however, the study area limits shall be mutually agreed upon by the developer, ~~his design~~ professional engineer, and City staff. These limits will usually result from initial discussion with staff. A vicinity map that shows the site, in relation to the surrounding transportation system, should be included.

5.1.2.1.2 Existing and Proposed Site Uses ~~.-~~ The existing and proposed uses of the site shall be identified by the applicable City zoning categories. When known, the specific intended use associated with the request shall also be identified, as multiple uses may be permitted by ordinance. The existing and proposed uses of the site should be identified in terms of the various zoning categories of the City. In addition, the specific use for which the request is made should be identified if known, since a number of uses may be permitted under the existing ordinances.

5.1.2.1.3 Existing and Proposed Uses in Vicinity of the Site. A complete description of the existing land uses in the vicinity of the site, as well as their current zoning and use, should be included.

The ~~report~~developer should also state ~~the~~proposed uses for vacant adjacent land ~~where in order that~~any proposed transition in uses is identified. This ~~latter item~~is especially important where large tracts of undeveloped and/or underdeveloped land are in the vicinity of the site, and within the prescribed study area. ~~Generally~~Generally, much of this information can be obtained from the initial meetings with the City's Planning ~~and Engineering~~ staff.

5.1.2.1.4 Existing and Proposed Roadways and Intersections. Within the study area, the ~~developer report~~ must describe existing roadways and intersections (geometrics and traffic signal control) as well as improvements contemplated by government agencies. This would include the nature of the improvement project, its extent, implementation schedule, and ~~funding source or the agency or funding source~~responsible.

5.1.2.2 Trip Generation and Design Hour Volumes.

5.1.2.2.1 A summary table listing each type of land use, the size involved, the average trip generation rates used (total daily traffic and a.m./p.m. peaks), and the resultant total trips generated shall be provided.

5.1.2.2.2 Trip generation will be calculated from the latest data contained within the *Institute of Transportation Engineers' Trip Generation Guide* (latest edition). ~~In the event that~~if data is not available for the proposed land use, the City must approve estimated rates prior to acceptance.

5.1.2.2.3 Site design hour volumes approximating the peak hour volume used to determine public improvements will be estimated by one of the following methods which are listed in order of preference:

- a. ———Traffic volume counts for existing uses.
- b. —Peak hour trip generation rates as published in the *ITE Trip Generation Guide* (latest edition).

5.1.2.3 Trip Distribution. The direction of approach for site-generated traffic will be presented in this section. The technical analysis steps, basic methods, and assumptions used in this work must be clearly stated.

5.1.2.4 Trip Assignment. This section will describe the utilization of study area roadways by site-generated traffic. The anticipated site traffic volumes must be combined with existing and projected area traffic volumes ~~in Section 5.1.2.5~~to describe mainline and turning movement volumes for future conditions with the site developed as proposed. Internal trips in excess of 10 percent will require analytical support to demonstrate how the higher figures were derived.

Nongenerated passerby traffic reductions in generation volumes may be considered if applicable. All estimates of trip distribution, assignment, and modal split are subject to review and approval by the City.

5.1.2.5 Existing and Projected Traffic Volumes.

5.1.2.5.1 Graphics should show:

- a. a.m. peak hour site traffic (in and out) including turning movements.
- b. p.m. peak hour site traffic (in and out) including turning movements.
- c. a.m. peak hour total, including site (in and out) and through traffic including turning movements for current conditions and the Metropolitan Planning Organization (MPO) planning year horizon or build-out.
- d. p.m. peak hour total including site (in and out) and through traffic including turning movements for current conditions and MPO planning year horizon or build-out.
- ~~d.e.~~ [The 24- hour volume counts of before and after development on all associated street segments. Future base volumes from the MPO traffic model can be obtained from City Engineering.](#)

5.1.2.5.2 All raw traffic count data (including hourly ADT and peak hour turning movements) and analysis worksheets shall be provided in the appendices. Computer ~~techniques~~ [software](#) and the associated printouts can be used as part of the report.

5.1.2.5.3 Build-out projections shall include major vacant properties around the proposed development as defined by the City. Volume projections for background traffic growth will be provided by the City, or a method for determining their volume will be recommended by the City.

5.1.2.5.4 All total daily traffic counts should be actual machine counts and not based on factored peak hour sampling. Latest available machine counts from the South Dakota Department of Transportation (SDDOT), the City, and other agencies may be acceptable if not more than two years old.

5.1.2.5.5 All traffic will be assigned to existing and planned facilities in a manner consistent with existing traffic patterns and approved by the City.

5.1.2.6 Capacity Analysis. A capacity analysis will be conducted for the street intersections at driveways for the proposed development. Within the limits of the previously defined study area, capacity analyses will also be conducted for street intersections. The a.m., p.m., and any other relevant peak period will be analyzed. Pedestrian, bicycles, and transit movements should also be

considered in the evaluation. Capacity calculations should also include an analysis for MPO planning year horizon or build-out conditions. Capacity analysis will be calculated using Synchro and SimTraffic software. The latest edition of the Highway Capacity Manual analysis report from Synchro shall also be included in the report. The default values for the capacity analysis in Synchro and SimTraffic will be in the default files provided by the City to the consultant electronically.

5.1.2.7 Traffic Signals

5.1.2.7.1 The need for new traffic signals shall be checked using the warrants in the *Manual on Uniform Traffic Control Devices*, latest edition. Traffic progression is of paramount importance. ~~Generally~~ **Generally**, a spacing of one-half mile for all signal-controlled intersections is desirable with the spacing not to drop below one quarter mile. This spacing is usually desirable to achieve good speed, capacity, **platooning**, and optimum signal progression.

5.1.2.7.2 To provide flexibility for existing conditions and ensure optimum signal progression, an approved traffic engineering analysis will be made to properly locate all proposed connecting access approaches that may require signalization if the access is more than 100' from a quarter mile **spacing** ~~access~~ **point**. A progression pattern will be established between two public intersections that bracket the proposed approach as chosen by the City. These bracketing intersections should be at least one mile apart, and be existing, or possible future signal locations. The progression patterns shall be established using ~~A.M. and P.M.~~ **a.m. and p.m.** peak platoon progression diagrams and an off-peak time space diagram that shows progression in both directions. The cycle lengths and splits used for capacity analysis shall be the same ones that are used for the progression patterns. Tru-Traffic, or a compatible software that saves files that can be opened by Tru-Traffic such as Synchro.csv files, shall be used to establish progression patterns. The City may choose not to signalize intersections shown to be less than desirable in the progression patterns.

5.1.2.7.3 The acceptable cycle lengths for capacity analysis and progression patterns are between 60 and 180 seconds with a travel speed of 40 mph, unless existing signal systems and speed limits govern usable cycle lengths and travel speeds. Left-turn phases shall have a minimum split of at least 10 seconds. Dual left-turn phases shall have a minimum split of at least 13 seconds. Thru-phases shall have a minimum split of at least 20 seconds, and no less than the time required for a pedestrian to cross the street in accordance with the latest edition of the *Manual of Uniform Traffic Control Devices*.

5.1.2.8 Level of Service ~~.~~ Level of Service (LOS) C during the peak hour will be the design objective for all new street components and intersections. Individual approaches shall be designed to at least LOS D for arterial street

approaches or a LOS E for collector/local/private street approaches, with no individual movement having a volume/capacity ratio of greater than 1.00. Existing corridors with established adjacent development shall be designed to LOS D. The design year will be the MPO planning year horizon or at build-out of the area. Levels of service are defined in *The Highway Capacity Manual*.

[If the Traffic Report includes impacts to a South Dakota highway or interstate interchange, the Traffic Reports shall adhere to the South Dakota Department of Transportation \(SDDOT\) LOS requirements. The Traffic Report shall also be coordinated with the SDDOT.](#)

5.1.2.9 Traffic Crashes and Other Considerations. Traffic crash data for affected street corridors may be required for the study. Where this is necessary, estimates of increased or decreased crash potential shall be evaluated for the development.

[Transit, pedestrian, and bicycle user impacts may also be required for the study. If a study area is expected to generate higher volumes of pedestrians and bicyclists, then these vulnerable users must be considered. Transit routes should also be evaluated as an option for the development transportation.](#)

5.1.2.10 Recommendations. ~~In the event that~~If the analysis indicates unsatisfactory levels of service on study area roadways, a description of proposed improvements to remedy deficiencies shall be included. These proposals would not include committed projects by the City or the SDDOT. In general, the recommendation section should include:

5.1.2.10.1 Proposed Recommended Improvements. This section shall describe the location, nature, ~~and~~ extent of proposed improvements to assure sufficient roadway capacity [and discussion on financial responsibilities of improvements for opening day of the development.](#)

5.1.2.10.2 Volume/Capacity Analysis at Critical Points. Another iteration of the volume/capacity analysis will be described, which demonstrates the anticipated results of making these improvements.

5.1.2.10.3 Levels of Service at Critical Points. As a result of the revised volume/capacity analysis presented in the previous section, levels of service for the highway system with improvements will be presented.

5.1.2.11 Conclusion. The last chapter of the report must be a clear, concise description of the study findings. It is anticipated that this concluding chapter will serve as an executive summary.

5.1.2.12 Revisions to Traffic Report. Revisions to the traffic report must be provided as required by the City. The ~~need to require~~requirement for revisions will be based on the completeness of the traffic report, the thoroughness of the impact evaluation, and the compatibility of the study with the proposed access and development plan.

5.1.2.13 Report Submittals-

The engineer shall submit [one electronic PDF bookmarked report of the two paper copies of the](#) Draft Traffic Impact Study to the City Engineering Division. In addition, the engineer shall submit the electronic files of Synchro and SimTraffic traffic data and output files.

The engineer shall submit ~~to the City~~ one [signed/stamped, electronic PDF bookmarked report of the paper copy and one electronic copy of the](#) Final Traffic Impact Study [to the City](#). The electronic copy shall include the report in PDF format and all the appendices in Synchro or SimTraffic format or a compatible format.

5.2 Access Control

5.2.1 General Access

Access in newly developing areas will follow these provisions. In areas being redeveloped, access will be determined as to the best fit based on traffic safety, existing conditions, future street improvements, and property development along with other considerations as appropriate.

A Sidewalk and Driveway Permit must be obtained from the City Engineer for any public or private access constructed to a [local or collector](#) public street. Access to streets or highways within the city limits under the jurisdiction of the South Dakota Department of Transportation (SDDOT) are also governed by requirements of the SDDOT [and are addressed in Chapter 8](#). In addition to obtaining a [sidewalk and driveway](#) permit from the City Engineer, a permit from the Area Engineer of the SDDOT must be obtained. Access shall be limited as dictated by ~~this the~~ City of Sioux Falls Design Standards. Any discrepancy between the SDDOT and the City of Sioux Falls regarding precedence of access design standards shall be jointly coordinated, reviewed, and approved. [All accesses to an arterial street shall adhere to the Chapter 8 Design Standards.](#)

Fire Department access to all buildings shall be provided and maintained during construction and upon completion of all improvements. Fire department access shall meet all requirements outlined in Fire Prevention Division Policy No. 05-01.

5.2.4 Definition of Terms for Access Control

Several terms are used herein which have a somewhat distinct meaning. For ~~the purpose of~~ clarity, the definitions of some of these terms are listed below.

5.2.4.1 Width of Curb Opening (W)—The width of curb opening measured at the throat of the driveway from the edge of pavement to the edge of pavement.

5.2.4.2 Property Line (P)—The distance measured along the property line from the nearest edge of the driveway to the property line.

5.2.4.3 Corner Clearance (C)—At an intersecting street the distance measured along the curb line from the end of the corner radius to the nearest edge of the curb opening.

5.2.4.4 Distance Between Double Drives/Accesses (D)—The distance measured along the curb line between the radii.

5.2.4.5 Frontage—The distance along the street right-of-way line of a single property or development within the property lines. Corner property at an intersection would have a separate frontage along each street.

5.2.4.6 Residential—Property used primarily for residential purposes such as single-family, two-family, and multifamily units.

5.2.4.6.1 Single-Family (SF) Residential—Single, detached family dwelling units or double bungalows or duplexes.

5.2.4.6.2 Multifamily (MF) Residential—Three or more attached dwelling units including townhouses, condominiums, and apartments.

5.2.2 Basic Principles for Curb Openings and Driveways

5.2.2.1 Arterial Street Access

5.2.2.1.1 Private residential access directly to arterial streets and any access to a principal arterial street shall be permitted only when the property in question has no other reasonable access to the general street system, or when denial of direct access to the arterial and alternative access to another roadway would cause traffic operation and safety problems as shown in a Traffic Report. Any access to arterials must adhere to City street standards as described in Chapter 8.

5.2.2.2 General Access

5.2.2.2.1 High Volume Access.—In general, when trip generation served by the driveway exceeds 100 vehicles per hour during the peak hour or the driveway accesses an arterial street, returns using a standard street return radius as set forth in Table 5.1 and Figure 5.1 will be required.

5.2.2.2.2 Access Points.—Access will not be approved for parking or loading areas that require backing maneuvers onto or from a public street right-of-way except for single-family or duplex residential uses on local and minor collector streets.

Table 5.1 Driveway Dimensions
(All Dimensions in Feet)

Arterial	Dimension Reference (See Figure 5.1)	Local			Collector			Arterial		
		Residential	Com-mercial	Industrial	Residential	Com-mercial	Industrial	Residential	Com-mercial	Industrial
Width ¹	W									
Minimum		12	15	20	12	15	20	15	15	20
Maximum		32 ²	36	40	30	36	40	36	36	40
Right-turn Radius	R									
Minimum		5	10	15	10	15	25	25	25	30
Maximum ³		15	20	30	25	50	50	30	50	50
Minimum Spacing ⁴										
From Property Line	P	0	R	R	0	R	R	R	R	R
From Street Corner	C	10-20	40	40	50	50	50	NA	NA	NA
Between Driveways	D	10 ⁵	25	25	35	50	50	NA	NA	NA
Angle ⁶	A	45°	70°	70°	45°	70°	70°			

¹ The minimum width of commercial driveways is intended to apply to one-way operation. In high pedestrian activity areas, such as in a central business district or in the same block with an auditorium, school, or library, the maximum basic width should be 30 feet. The width shown applies to rural routes and most City streets including neighborhood business, residential, and industrial streets. The width is intended to be measured along the right-of-way line, in most instances, at the inner limit of a curbed radius or between the line of the radius and the near edge of a curbed island at least 50 square feet in area.

² ~~Maximum width on bulb of cul-de-sac shall be 24 feet. Maximum width of 12 feet per garage stall up to a total maximum of 36 feet except when located on a cul-de-sac bulb where the maximum width is 24 feet.~~

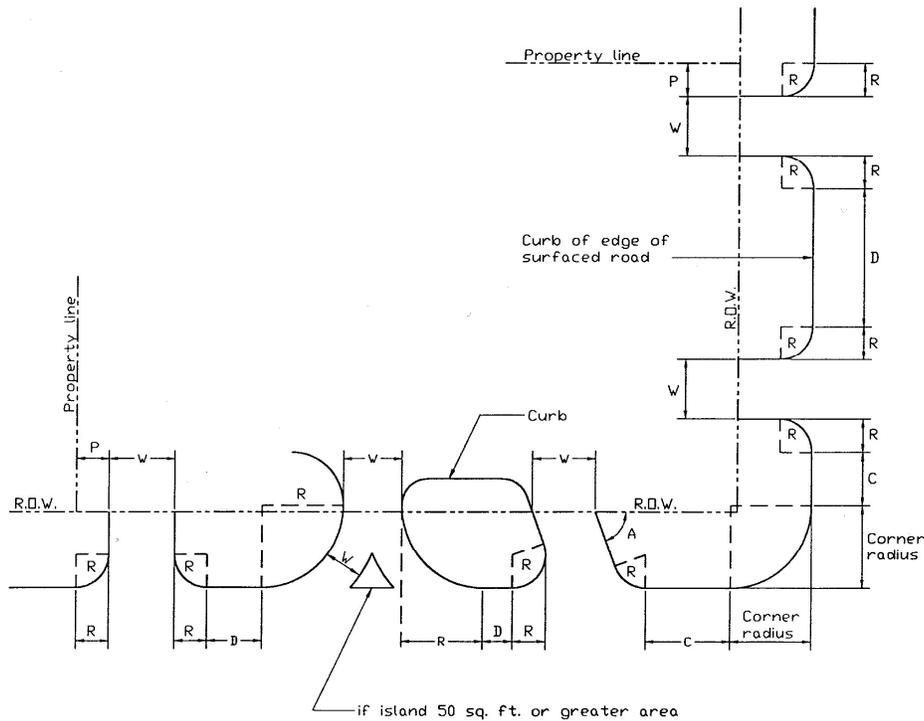
³ On the side of a driveway exposed to entry or exit by right-turning vehicles. In high pedestrian activity areas, the radii should be half the values shown. The maximum radii for major generator driveways shall be designed in accordance with *A Policy on Geometric Design of Highways and Streets*, published by AASHTO latest edition.

⁴ Measured along the curb or edge of pavement from the roadway end of the curb radius. In high pedestrian activity areas, the minimum spacing between driveways should be 5 feet.

⁵ Minimum space between driveways may be reduced to 3 feet on one side on local streets only at the discretion of the City Engineer.

⁶ Minimum acute angle measured from edge of pavement, and generally based on one-way operation. For two-way driveways, and in high pedestrian activity areas, the minimum angle should be 80 degrees.

Figure 5.1 Driveway Dimensions



5.2.2.2.3 Standards. Every property that accesses the street shall have a driveway. Driveways shall be constructed in accordance with the City of Sioux Falls Standard Plates.

5.2.2.2.4 Existing and Future Demands. The opening or driveway width shall be adequate to handle properly the anticipated traffic volume and character of traffic, as well as being within the limits specified for the type of property development. The controls established for curb openings and driveways shall apply to existing streets as well as new streets that may be developed in the future.

5.2.2.2.5 Utility Conflicts. Any adjustments which must be made to utility poles, street light standards, fire hydrants, catch basins or inlets, traffic signs and signals, or other public improvements or installations which are necessary as the result of the curb openings or driveways shall be accomplished without any cost to the City.

5.2.2.2.6 Access Signs. Driveways designated for entrance-only or exit-only use shall be signed by the property owner, at the owner's expense, subject to approval by City Planning and Development Services. All signs shall conform to the location, height, and legend requirements of the Manual on Uniform Traffic Control Devices (MUTCD). Driveway approaches, whereby the driveway is to serve as an entrance only or as an exit only, shall be appropriately signed by, and at the expense of, the

~~property owner subject to approval of City Planning and Building Services. Sign location, height, and legend must be in accordance with the *Manual on Uniform Traffic Control Devices (MUTCD)*.~~

5.2.2.2.7 Abandoned Driveways. Any curb opening or driveway which has been abandoned shall be removed and the street restored by the property owner to the satisfaction of the City Engineer.

5.2.3 General Requirements

5.2.3.1 Number of Openings

5.2.3.1.1 Single-Family Residential—In general, each single-family residential property shall be limited to one access point. However, where houses are located on corner lots or have extra wide frontage, more than one access point may be permitted. Applicable zoning setback requirements must be followed.

5.2.3.1.2 Multi-Family Residential—In general, access shall be determined by information provided by the owner/developer in a Traffic ~~Impact~~ Report and/or by comments generated during the City's review and acceptance of that report.

5.2.3.1.3 Commercial/Industrial—In general, access to commercial and industrial property shall be limited to the requirements as set forth in Chapter 8 of the Engineering Design Standards and shall be based on the street classification described by the 2025-current version of the Major Street Plan that is kept in the Office of the City Engineer. For commercial/industrial property located on a corner of an arterial street, access may be restricted to a side street only. Access may also be restricted if use of such access would be precluded by existing left turn lanes or other traffic control devices.

5.2.3.2 Access to Roadways with No Curb and Gutter

Private drive access to local, collector, or arterial streets that have no curb and/or gutter improvements shall be constructed to meet the following requirements:

5.2.3.2.1 The private drive shall extend from right-of-way line to the edge of the existing driving surface and shall be constructed of: ~~(a) an 8-inch-thick~~ 8-inch-thick compacted aggregate base material, or if paved, ~~(b) a minimum 4-inch-thick asphalt pavement over 6-inch-thick aggregate base material; i.e., minimum acceptable roadway pavement design.~~

5.2.3.2.2 Access shall be governed by the driveway criteria.

5.2.3.2.3 A culvert properly sized for the ditch flow shall be installed at the established roadside ditch flowline beneath the private drive access. Minimum size for the culvert shall be 15 inches. Culverts shall have a precast concrete-sloped end section or cast-in-place concrete headwall. If

a cast-in-place headwall is built, it shall have a maximum slope of 4:1 on any exposed face. No vertical headwalls will be allowed.

5.2.3.2.4 A sketch plan of the installation must be submitted with the access permit application. No construction permit will be issued until the access and its construction plan or sketch are approved by the Office of the City Engineer.

5.2.3.3 ~~Amount of Curb Opening Permitted~~ Driveway width shall comply with Table 5.1.

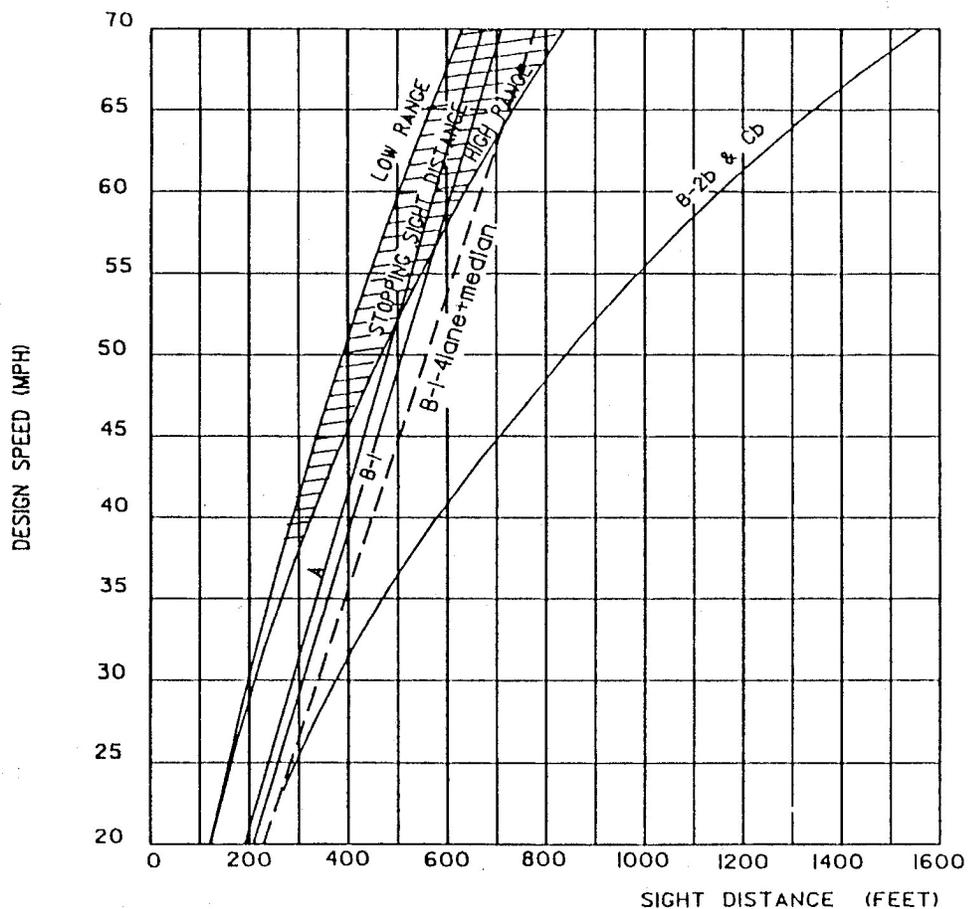
5.2.3.4 Mutual Access On commercial, industrial, and multifamily developments, mutual use of access to streets is encouraged and may be necessary to meet driveway spacing requirements. Where used, mutual access will comply with City ordinance and will be shown on plans for approval prior to construction or change of use.

5.3 Access Design

5.3.1 Driveway Spacing

Where lots are large enough, the center of driveways not in alignment will normally be offset a minimum of 150 feet for all commercial/multifamily properties. Greater distances may be required if left-turn storage lanes require such on arterial streets. Minimum sight distance shall be provided at all access points as stated in the current edition of AASHTO as shown in Figure 5.2, which applies to both city street and driveway intersections.

Figure 5.2: Intersection Distance at At-Grade Intersection [Delete this table](#)



A—Sight distance for a passenger vehicle crossing a two-lane highway from a stop (see diagram).
A—SIGHT DISTANCE FOR P VEHICLE CROSSING TWO-LANE HIGHWAY FROM STOP (SEE DIAGRAM).

B-1—Sight distance for a passenger vehicle turning left into a two-lane highway across a vehicle approaching from the left (see diagram).
B-1—SIGHT DISTANCE FOR P VEHICLE TURNING LEFT INTO TWO-LANE HIGHWAY ACROSS P VEHICLE APPROACHING FROM LEFT (SEE DIAGRAM).

B-1-4 Lane+median—Sight distance for a passenger vehicle turning left into a four-lane highway across a vehicle approaching from the left (see diagram).SIGHT DISTANCE FOR P VEHICLE TURNING LEFT INTO FOUR LANE HIGHWAY ACROSS P VEHICLE APPROACHING FROM LEFT (SEE DIAGRAM).

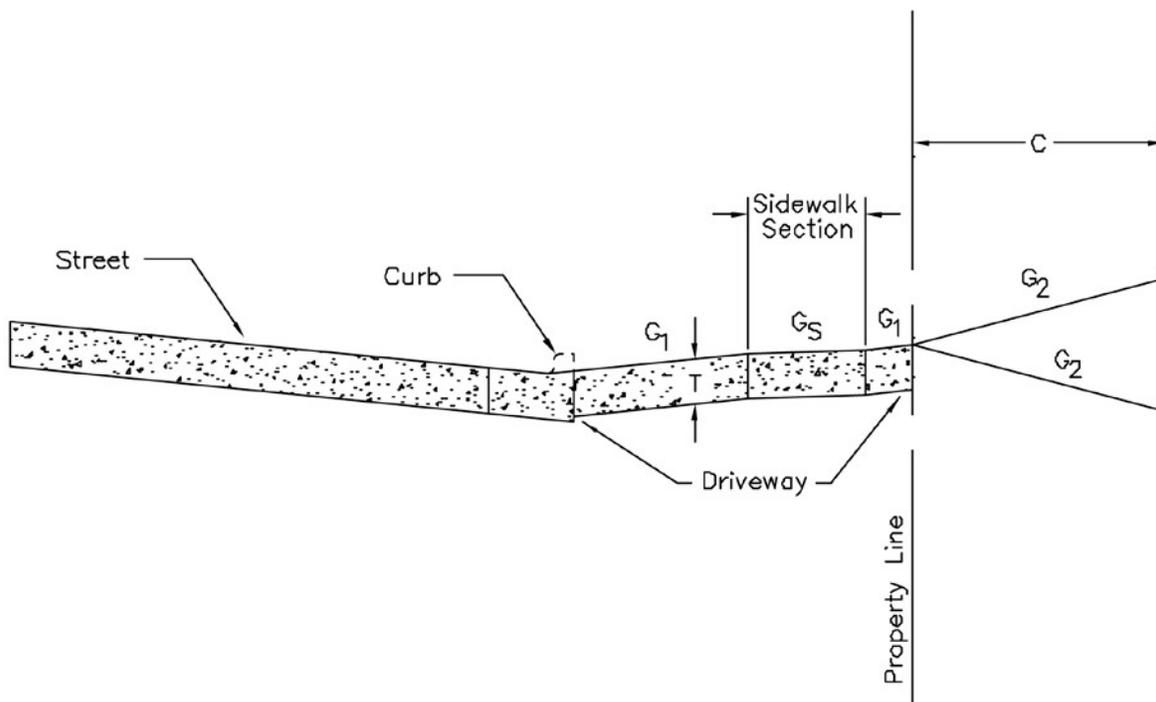
B-2b—Sight distance for a passenger vehicle turning left into a two-lane highway and accelerating to 85% of design speed without being overtaken by a vehicle approaching from the right (see diagram).B-2b—SIGHT DISTANCE FOR P VEHICLE TO TURN LEFT INTO TWO-LANE HIGHWAY AND ATTAIN 85% OF DESIGN SPEED WITHOUT BEING OVERTAKEN BY A VEHICLE APPROACHING FROM THE RIGHT REDUCING SPEED FROM DESIGN SPEED TO 85% OF DESIGN SPEED (SEE DIAGRAM).

C-b—Sight distance for a passenger vehicle turning right into a two-lane highway and accelerating to 85% of design speed without being overtaken by a vehicle approaching from the left (see diagram).Cb—SIGHT DISTANCE FOR P VEHICLE TO TURN RIGHT INTO TWO-LANE HIGHWAY AND ATTAIN 85% OF DESIGN SPEED WITHOUT BEING OVERTAKEN BY A VEHICLE APPROACHING FROM THE LEFT AND REDUCING FROM DESIGN SPEED TO 85% OF DESIGN SPEED.

5.3.2 Driveway Design

Driveway sectional details are shown in Figure 5.3 with design requirements listed in Table 5.2.

Figure 5.3 Driveway Grades



*All sidewalk grades (Gs) shall be 2.0% maximum.

Table 5.2

Type of Driveway	Minimum Thickness	Grade (G1)		Grade (G2)		Control Distance (C)
		Min.	Max.	Min.	Max.	
Low Volume Residential	6"	+5.0%	+8.0%	±0.5%	±13%	20'
Low Volume Commercial/Industrial	7"	±0.5%	±6%	±0.5%	±6%	40'
High Volume	7"	±0.5%	±3%	±0.5%	±3%	40'

Driveways may be required to be constructed with turning restrictions. For example, designs as a right out only, right in only, right in-right out, partial $\frac{3}{4}$ access, are some options that can be used. These designs will be recommended and approved by the City Engineer and shall adhere to best practices and design requirements as stated in the current edition of AASHTO.

The following sketches are the recommended minimum design for limited movement driveways. Acceleration and deceleration lanes may be required to be incorporated into the designs. The islands are raised with vertical curb. The ends of the islands should typically be provided with 2-foot radii. ~~DELETE THE FIGURES 5.4 THROUGH 5.8.~~

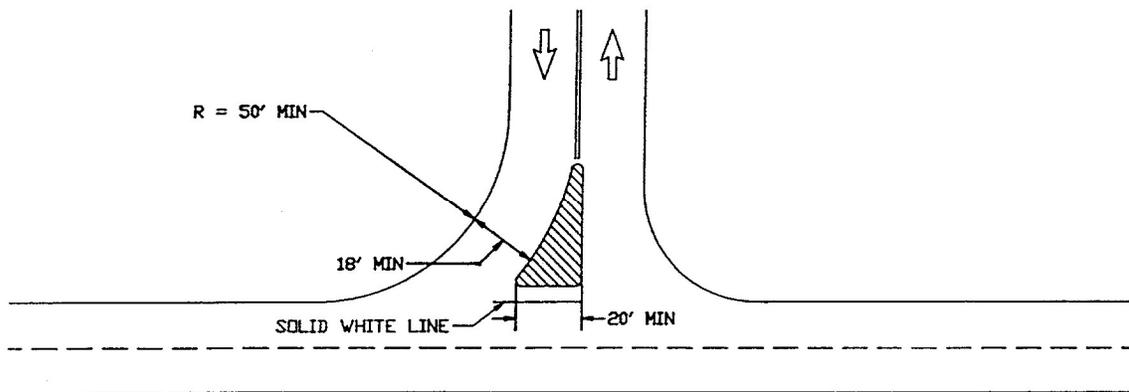


Figure 5.4: Right-In, Right-Out, Left-In Driveway Design

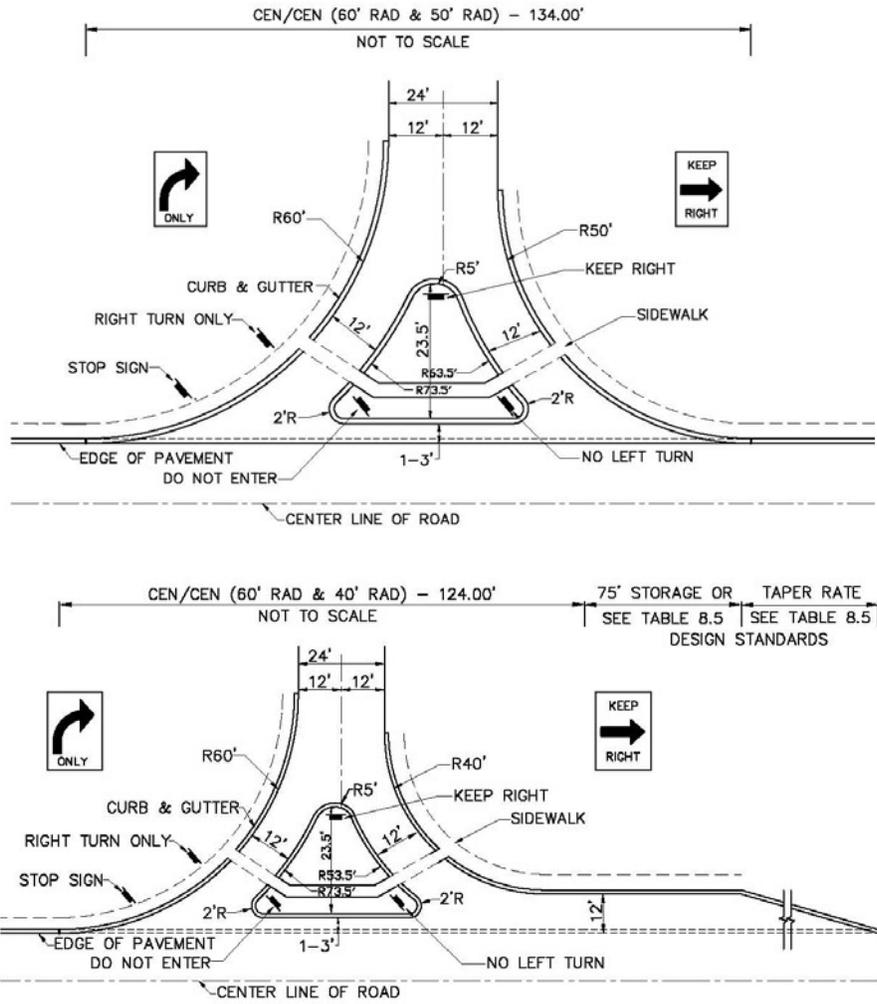


Figure 5.5: Right-In, Right-Out Driveway Designs

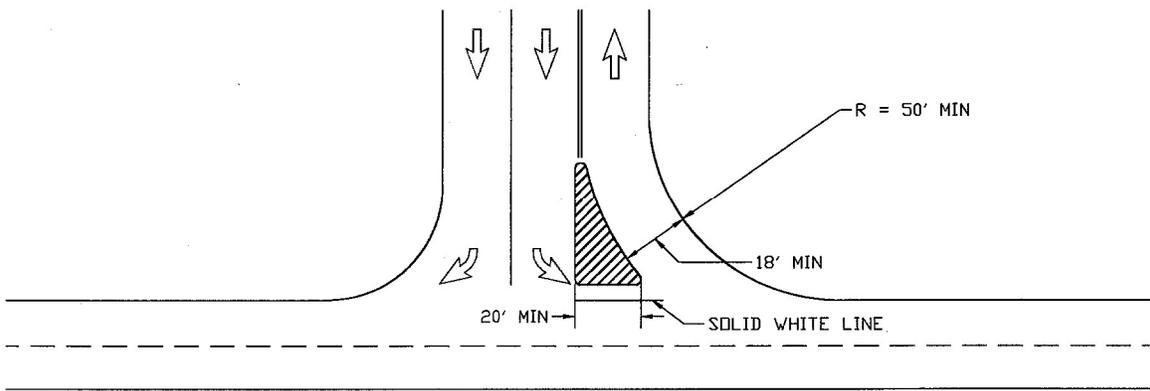


Figure 5.6: Right-In, Right-Out, Left-Out Driveway Design

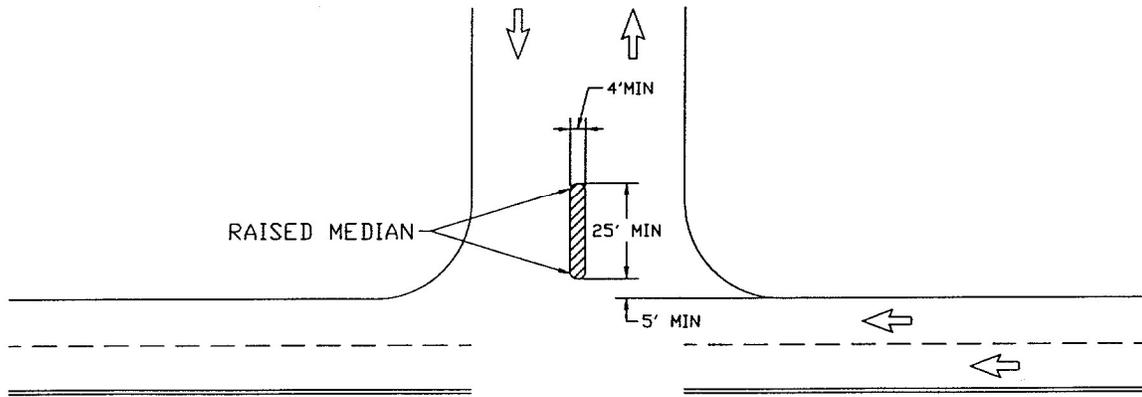


Figure 5.7: Driveway Design with Median Divider

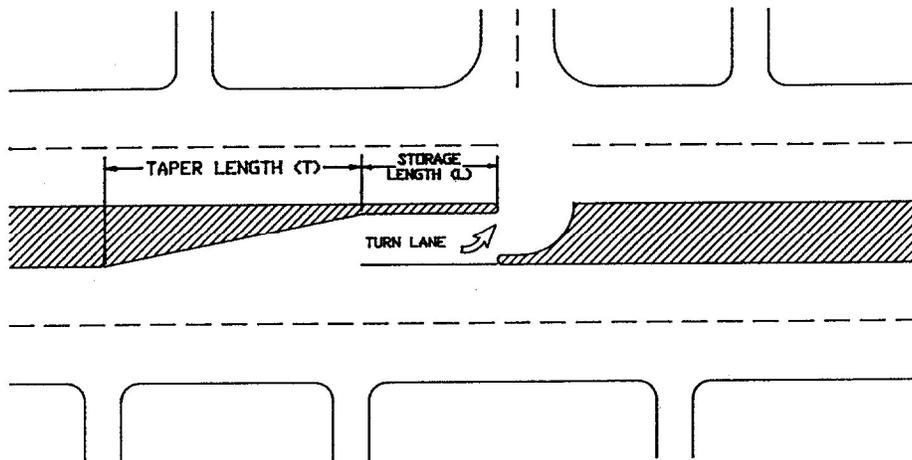


Figure 5.8: Median Design to Restrict Existing Left Turns

5.4 Off-Street Parking Area

5.4.1 General. In accordance with City Zoning Ordinances [160.550](#) and [160.556](#) ~~15.55.010~~ and [15.55.070\(e\)](#), the following guidelines regarding the design of off-street parking areas shall be followed.

5.4.2 Minimum Stall Width. The minimum stall width shall be 8-1/2 feet, except [stalls designated for](#) compact vehicles. Compact vehicle minimum stall width shall be 7-1/2 feet.

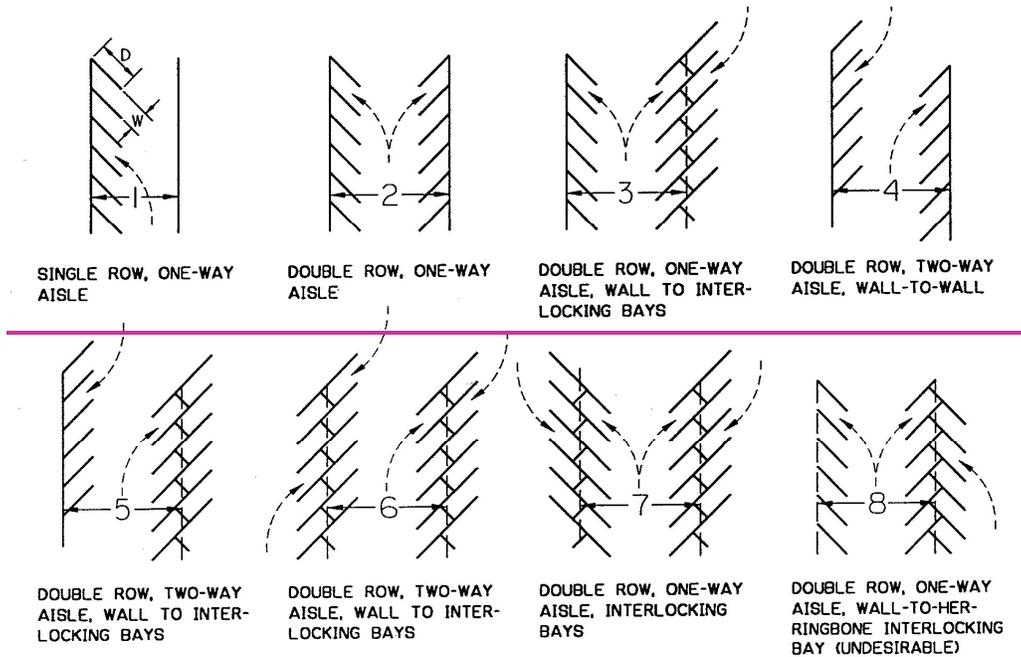
5.4.3 Compact Vehicles. A maximum of 33 percent of total required parking stalls may be designated specifically for compact vehicles. When an area is designed for compact vehicles only, the area shall be adequately signed to inform drivers of the exclusion.

5.4.4 Parking Dimensions. Minimum module depths shall be as shown on Figure 5.9 and in Table 5.3.

5.4.5 Backing Into Street Not Allowed. Parking spaces for a parking lot shall be arranged so that vehicles are not required to back into the street to exit a lot, except for single-family and duplex dwellings. The spaces shall be so arranged so that no vehicle will be required to be backed into the street in order to exit the lot except for single-family or duplex dwelling units.

5.4.6 Backing Over Sidewalk Not Allowed. Parking spaces for a parking lot shall be arranged so that vehicles can exit without backing over a public sidewalk. The spaces shall be so arranged so that no vehicle will be required to be backed over a public sidewalk in order to exit the stall.

Figure 5.9 Parking Area Layout



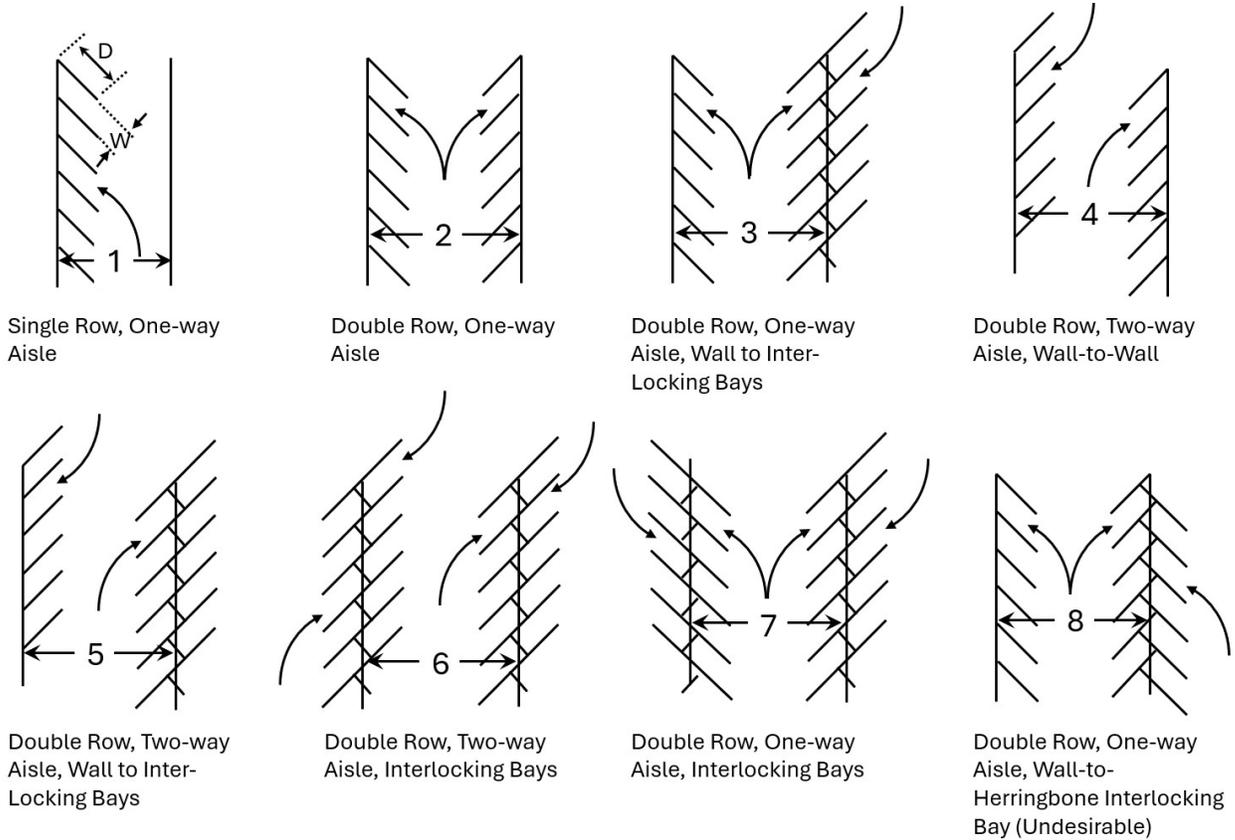


Table 5.3
Minimum Parking Lot Design Requirements
 (All Dimensions in Feet)

PARKING ANGLE (DEGREES)	STALL DEPTH "D" (FEET)	STALL WIDTH "W" (FEET)	MODULE DEPTH (FEET)							
			1	2	3	4	5	6	7	8
45	16*	7.5*	26	41	40	52	50	50	39	39
45	18	8.50	30	48	45	58	54	51	42	42
45	18	9.00	30	48	45	58	54	51	42	42
45	18	9.50	30	48	45	58	54	51	42	42
45	18	10.00	30	48	45	58	54	51	42	42
60	17*	7.5*	29	46	45	57	55	53	43	43
60	20	8.50	37	57	55	60	58	56	53	53
60	20	9.00	36	56	54	60	58	56	52	52
60	20	9.50	34	54	52	60	58	56	50	50
60	20	10.00	33	53	51	60	58	56	50	50
75	17*	7.5*	33	49	49	50	49	49	49	49
75	20	8.50	41	61	60	61	60	59	59	59
75	20	9.00	40	60	59	60	59	58	58	58
75	20	9.50	39	59	58	59	58	57	57	57
75	20	10.00	38	58	57	58	57	57	57	57
90	16*	7.5*	35	50	49	50	50	50	50	50
90	19	8.50	45	64	64	64	64	64	64	64
90	19	9.00	43	62	62	62	62	62	62	62
90	19	9.50	42	61	61	61	61	61	61	61
90	19	10.00	41	60	60	60	60	60	60	60

* - COMPACT CARS ONLY

Parking Angle (Degrees)	Stall Depth "D" (Feet)	Stall Width "W" (Feet)	Module Depth (Feet)							
			1	2	3	4	5	6	7	8
45	16*	7.5*	26	41	40	52	50	50	39	39
45	18	8.5	30	48	45	58	54	51	42	42
45	18	9	30	48	45	58	54	51	42	42
45	18	9.5	30	48	45	58	54	51	42	42
45	18	10	30	48	45	58	54	51	42	42
60	17*	7.5*	29	46	45	57	55	53	43	43
60	20	8.5	37	57	55	60	58	56	53	53
60	20	9	36	56	54	60	58	56	52	52
60	20	9.5	34	54	52	60	58	56	50	50
60	20	10	33	53	51	60	58	56	50	50
75	17*	7.5*	33	49	49	50	49	49	49	49
75	20	8.5	41	61	60	61	60	59	59	59
75	20	9	40	60	59	60	59	58	58	58
75	20	9.5	39	59	58	59	58	57	57	57
75	20	10	38	58	57	58	57	57	57	57
90	16*	7.5*	35	50	49	50	50	50	50	50
90	19	8.5	45	64	64	64	64	64	64	64
90	19	9	43	62	62	62	62	62	62	62
90	19	9.5	42	61	61	61	61	61	61	61
90	19	10	41	60	60	60	60	60	60	60

* Compact Cars Only

5.5 On-Street Diagonal Parking in Urban Villages

5.5.1 General

~~On-street diagonal parking is generally prohibited on City streets except by ordinance. The Shape Sioux Falls guidelines encourage complete streets principles, including enhanced pedestrian and bicycle facilities, creation of urban villages, and traffic calming. New developments may request diagonal on-street parking as part of a Planned Unit Development rezoning. Such parking may be permitted only by ordinance and must be included in the Planned Unit Development Rezoning package. Diagonal parking requests with site redevelopments must be permitted by City Engineer and approved by resolution as a part of the Site Plan approval process. Generally, on-street diagonal parking is not allowed on City streets except by City Ordinance approval. The Shape Sioux Falls guidelines adopted by the City of Sioux Falls, encourages a more complete streets implementation. As part of the complete streets philosophy, enhanced pedestrian and bicycle facilities, creating urban villages, and calming streets are encouraged. New developments may request diagonal on-street parking as part of their planned development. The diagonal parking will be allowed and approved by Ordinance only and should be submitted as part of the Planned Development Rezoning package.~~

5.5.2. Minimum Standards.

5.5.2.1 Vehicle Pulling Into the Diagonal Stall

1. [Diagonal parking requests with site redevelopments must be permitted by City Engineer and approved by resolution as a part of the Site Plan approval proces](#)
- 4.2. Minimum street width is 32 feet, [measured from the](#) face of curb to centerline of ~~the driving lanes~~[street](#). Minimum right-of-way width is 40 feet from centerline of the street. Minimum stall width is 9 feet. The angle shall be 60 degrees or less. [For angled parking less than 60 degrees, consideration may be given to reducing the required 32 feet street width.](#)
- 2.3. Diagonal parking may be allowed on local or collector streets. No diagonal parking will be allowed on arterial streets.
- 3.4. A maintenance agreement is required to be executed between the developer and the City [and filed with the register of deeds within 15 days of the effective date of the resolution approving angle parking](#)~~before the Development Engineering Plans are approved.~~
- 4.5. The diagonal parking will not be metered.

5.5.2.2 Vehicle Backing Into the Diagonal Stall

1. [Diagonal parking requests with site redevelopments must be permitted by City Engineer and approved by resolution as a part of the Site Plan approval proces](#)
- 4.2. Minimum street width is 33 feet, [measured from](#) face of curb to centerline of ~~street~~[the driving lanes](#). Minimum right of way width is 40 feet from the centerline of the street. Minimum stall width is 9.5 feet. The angle shall be 60 degrees or less. [For angled parking less than 60 degrees, consideration may be given to reducing the required 33 feet street width.](#)
- 2.3. Diagonal parking may be allowed on local or collector streets. No diagonal parking will be allowed on arterial streets.
- 3.4. A maintenance agreement is required to be executed between the developer and the City [and filed with the register of deeds within 15 days of the effective date of the resolution approving angle parking](#)~~before the Development Engineering Plans are approved.~~
- 4.5. The diagonal parking will not be metered.
- 5.6. Bicycle lanes may be accommodated with this street section.



CITY OF
SIOUX FALLS

**IRAB March Meeting
2026 Construction Outlook**

(www.siouxfalls.gov/construction)

March 4, 2026

MAINTAIN
ABOVE AVERAGE
STREET
CONDITION
INDEX

70

EFFICIENT SNOW
REMOVAL SERVICES

<48 HOURS

FULLTIME POSITIONS

123 ↑ 0

PART-TIME POSITIONS
(FULLTIME EQUIVALENTS)

20

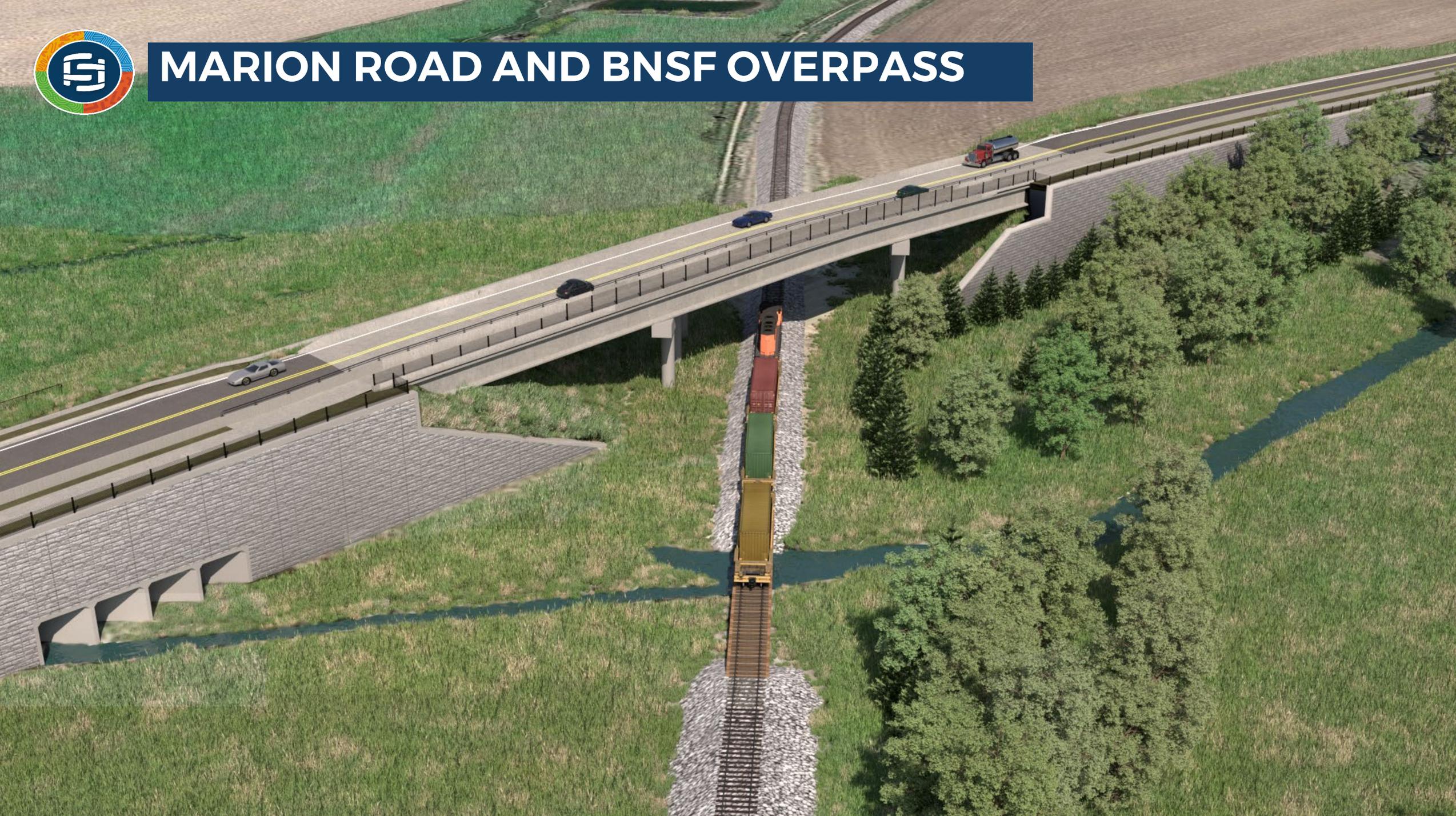
2026 BUDGET BY MAJOR DRIVERS

	Total Budget
Capital	\$65.11M
Total Personnel	\$16.80M
Fleet Equipment, Fuel, & Equipment Rentals	\$10.77M
Street Light Electricity & Repairs	\$2.00M
Sidewalk Repair Program	\$1.51M
Traffic Signs, Signals, & Markings	\$1.36M
Street Repairs & Materials	\$1.04M
Technology	\$1.00M
Street Winter Treatment	\$1.00M
Facility Maintenance, Utilities, & Insurance	\$0.59M
Snow Removal Contractors	\$.52M
Studies & Inspections	\$.30M
Operational Resources	\$.19M
Tools, Gear, & Shop Supplies	\$.18M
Training & Development	\$.06M

Recalibration—Fleet equipment, equipment rentals, street winter treatment, snow removal contractors (\$1.2M)



MARION ROAD AND BNSF OVERPASS





85TH ST. & I-29 DDI INTERCHANGE



CITY OF
SIOUX FALLS



ARROWHEAD & VETERANS PARKWAY



VIEW TO NORTHEAST
ARROWHEAD PARKWAY - PHASE 2B
November 14, 2024



South Veterans Parkway

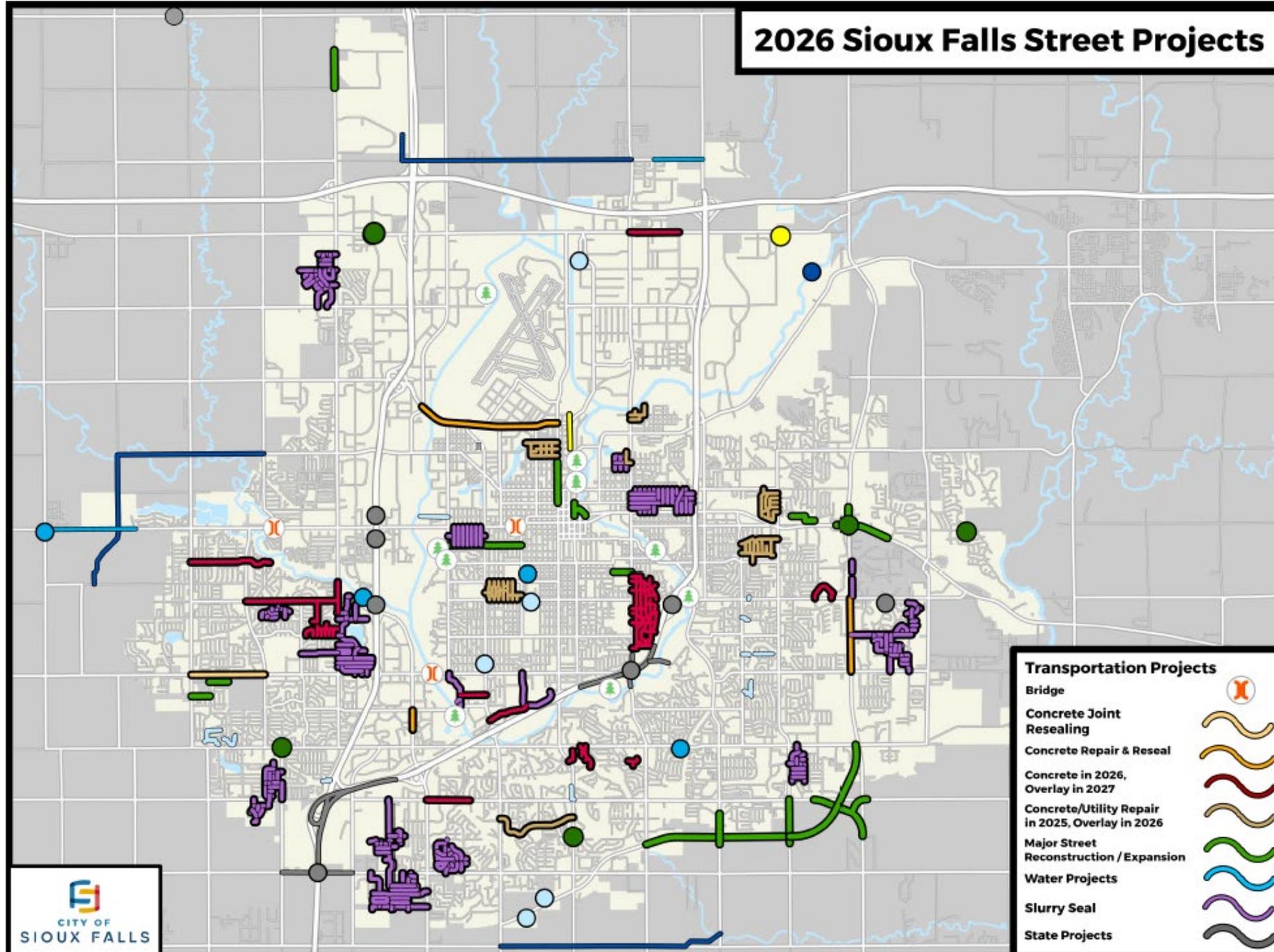


CLIFF AVENUE & I-229 INTERCHANGE



Sioux River

2026 Sioux Falls Street Projects



CITY OF SIOUX FALLS

Sioux Falls MPO
2026-2029 TIP

Construction Projects

State	County	City	Year
			2026
			2027
			2028
			2029
	State Project		
	County Project		
	City Project		
	State-City Project		
	MPO Boundary		

